CITY OF ROSEBURG PLANNING COMMISSION

Monday, April 5 at 7:00 pm Electronic Meeting

Electronic Meeting

Public Access: Facebook Live at www.Facebook.com/CityofRoseburg

City website at: https://www.cityofroseburg.org/your-government/commissions/planning-

commission/videos

AGENDA

- 1. CALL TO ORDER
- 2. ROLL CALL

Shelby Osborn, Chair Daniel Onchuck Victoria Hawks Christy Palmini Stephanie Newman

Ron Sperry

Jaime Yraguen

- 3. APPROVAL OF MINUTES
 - A. February 1, 2021 Planning Commission Meeting
- 4. ELECTION OF OFFICERS
- 5. AUDIENCE PARTICIPATION: See Information on the Reverse
- 6. PUBLIC HEARING
 - A. SR-21-027 Site Plan Review for Winco Foods [Quasi-Judicial]
 - B. LUDR-21-001 Land Use Regulation Text Amendments [Legislative Amendment]
- 7. BUSINESS FROM STAFF
 - A. Director's Report
- 8. BUSINESS FROM THE COMMISSION
- 9. **NEXT MEETING** May 3, 2021
- 10. ADJOURNMENT

The agenda packet is available on-line at:

http://www.cityofroseburg.org/your-government/commissions/planning-commission/

The Planning Commission meetings are available to view on the City website the next day at: https://www.cityofroseburg.org/your-government/commissions/planning-commission/videos

AMERICANS WITH DISABILITIES ACT NOTICE

Please contact the office of the City Recorder, Roseburg City Hall, 900 SE Douglas Avenue, OR 97470 (Phone 541-492-6700) at least 48 hours prior to the scheduled meeting time if you need an accommodation. TDD users please call Oregon Telecommunications Relay Service at 1-800-735-2900.

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AUDIENCE PARTICIPATION INFORMATION

The Roseburg Planning Commission welcomes and encourages participation by citizens at all meetings. To allow the Commission to deal with business on the agenda in a timely fashion, we ask that anyone wishing to address the Commission follow these simple guidelines:

To provide comment during virtual meetings, contact the Community Development Department by phone (541-492-6750) or email cdd@cityofroseburg.org by 4:00 p.m. the day of the meeting. Please indicate at that time if ADA accommodations are requested. Provide your name, address, phone number, whether or not you are a resident of the City of Roseburg and which item on the agenda you wish to speak. You will then be provided with a link and phone number to join the Commission meeting. Log or call in prior to the start of the meeting using the link or phone number provided.

- When accessing the meeting through the **ZOOM link**, you will be asked to enter your email and name. After entering your email and name, click "Join Webinar" to join the meeting as an attendee.
- When accessing the meeting through the phone, call the number provided.
- All attendees will be held in a "waiting room" until called on to speak.

Persons addressing the Commission must state their name and address for the record, including whether or not they are a resident of the City of Roseburg. All remarks shall be directed to the entire Commission. The Commission reserves the right to delay any action requested until they are fully informed on the matter.

PROVIDING COMMENTS

If you join the meeting via Zoom, please <u>select the "raise hand" button</u> when the Chair calls for speakers. You will be moved out of the "waiting room" to speak. After you have provided your comments, you will be moved back into the "waiting room".

If you join the meeting on the phone number you provided to the Community Development Department, you will be brought into the meeting to speak when the Chair calls for speakers, then moved back to the "waiting room".

CITIZEN PARTICIPATION - AGENDA ITEMS

Anyone wishing to speak regarding an item on the agenda may do so when the Commission addresses that item.

Agenda items typically begin with establishing those who have party status, (to be explained by the Chair), a report from staff, followed by Commission questions to staff, then the applicant, along with anyone they wish to call as a witness on their behalf will be called to speak, followed by those with party status. After all initial testimony is completed there will be an opportunity for rebuttal. Everyone addressing the Commission is subject to questioning. After the hearing portion of the item is completed, the Commission will discuss the matter with a motion for consideration being presented and acted on.

Once final action is taken on Quasi-Judicial matters, the action of the Commission can be appealed to City Council within 14 calendar days of the decision by filing a Notice of Review with the Community Development Department. Action on Legislative matters is typically a recommendation to City Council and will be forwarded to them for final consideration.

CITIZEN PARTICIPATION - NON-AGENDA ITEMS

Anyone wishing to speak regarding an item not on the evening's agenda, may do so under "Audience Participation."

Persons addressing the Commission must state their full name and address, including whether or not they are a resident of the City of Roseburg, for the record. All remarks are to be directed to the Commission. For items not on the agenda:

- Presentation should be brief and on a topic of interest to the Planning Commission, such as a general land use matter.
- These presentations are reserved for new material which has not been previously considered.

The Commission will not be taking action on any item presented under Audience Participation and if needed will provide direction to staff for appropriate follow-up.

For further details or information please contact the Community Development Department Monday through Friday, 8-12 and 1-4:30 p.m., closed 12-1 p.m. Tel: 541-492-6750, or e-mail cdd@cityofroseburg.org. Roseburg City Hall, 900 SE Douglas Avenue, Roseburg OR 97470.

CITY OF ROSEBURG PLANNING COMMISSION MINUTES February 1, 2021

CALL TO ORDER

Vice Chair Osborn called the regular meeting of the Roseburg Planning Commission to order at 7:00 p.m. on Monday, February 1, 2021 via Zoom.

ROLL CALL

<u>Present</u>: Vice Chair Shelby Osborn, Commissioners Victoria Hawks, Stephanie Newman, Dan Onchuck, Christy Palmini, Ron Sperry and Jaime Yraguen.

Others present: Associate Planner John Lazur and Department Technician Chrissy Matthews.

APPROVAL OF MINUTES

Commissioner Yraguen moved to approve the November 2, 2020 minutes as submitted. The motion was seconded by Commissioner Onchuck and approved with the following votes: Vice Chair Osborn, Commissioners Hawks, Newman, Onchuck, Palmini, Sperry and Yraguen voted yes. No one voted no.

ELECTION OF OFFICERS

Commissioner Onchuck moved to nominate Shelby Osborn for Chair. The motion was seconded by Commissioner Hawks. Commissioner Osborn accepted the nomination. The nomination was approved with the following votes: Commissioners Hawks, Newman, Onchuck, Osborn, Palmini, Sperry and Yraguen voted yes. No one voted no.

Commissioner Yraguen moved to nominate Ron Sperry for Vice Chair. The motion was seconded by Commissioner Palmini. Commissioner Sperry accepted the nomination. The nomination was approved with the following votes: Commissioners Hawks, Newman, Onchuck, Osborn, Palmini, Sperry and Yraguen voted yes. No one voted no.

Vice Chair Osborn asked staff if any public comments were received prior to the meeting. Mr. Lazur stated no public comments or questions were received.

AUDIENCE PARTICIPATION – None

PUBLIC HEARING

No Public Hearings were scheduled.

BUSINESS FROM STAFF -

- A. 2020 Year End Report Mr. Lazur briefed the Commission on the Community Development Department 2020 Year End Report, highlighting the following:
 - Approximately 244 total Site Review applications were processed
 - Approximately 225 total Building Permits were processed
 - 139 Building Permits were issued for commercial development including:
 - The Landing memory care and assisted living facility
 - Evergreen Family Medical Center converted a 30,000 square foot vacant commercial building into the new medical facility
 - o Hanna Heritage Five Guys, Spectrum and coming soon, Crumbl Cookies

- o Red Barn new location on W Umpqua Street
- Lithia Roseburg Auto Center added a 9,500 square foot maintenance shop
- Mountain Mike's Pizza and Cascade Coffee
- Aviva Health has begun grading for construction of a 30,000 square foot medical facility expansion
- Gecko RV & Boat Storage Phase 3

Significant multi-family dwelling projects currently under construction include:

- Deer Creek Village apartments 68-units of affordable housing
- o Oak Springs apartments 89 units of market rate housing
- o Townhouse subdivision 14 units
- Renovation of the old Valley Hotel 34 units of Single Room Occupancy (SRO) style apartment units
- 36 Land Use Applications were processed
- 45 pre-application conferences

B. 2021 Projects. Mr. Lazur highlighted the following projects for 2021:

- Bike Routes Plan (TGM Grant) In September of 2020, the City was awarded a Transportation Growth Management (TGM) grant through ODOT and DLCD, enabling the City to establish a new Bikes Route Plan. The City is currently working with ODOT to scope the project and hire a consultant. The purpose of the plan will be to implement a designated route system throughout the community. A Planning Commission member will be asked to be on the Planning Advisory Committee, when the time comes.
- Middle Housing (HB2001) Code Update Project In September of 2019, City Council amended the Roseburg Comprehensive Plan by adopting an updated Housing Needs Analysis (HNA). The purpose of the HNA was to evaluate the City's ability to meet the housing demands for the next 20-year planning period. Code updates involving middle housing options include duplexes, triplexes, quadplexes, cottage clusters and townhouses.
- Urban Growth Boundary (UGB) Swap In November of 2020, a traffic engineer was hired by the City to complete a Traffic Impact Study (TIS) to determine how a possible UGB swap may affect traffic within the area. It is anticipated this study will be completed in the spring of 2021 and an application for the swap could occur in the summer/fall of 2021.
- Hwy 138 Design Plan Grant funding was obtained through ODOT to study the Diamond Lake Blvd corridor and Hwy 138 to achieve safer travel for all modes of transportation due to the increase in housing and the Urban Renewal area designation. A consultant is in the process of being hired. The project is anticipated to start in spring. A Planning Commission member will be asked to be on the Planning Advisory Committee, when the time comes.

Long Range Planning projects completed in 2020 include:

Code Text Amendments. City Council adopted amendments revising the Land Use and Development Regulations as a result of an audit conducted by the City Recorder, in which a number of outdated references to Oregon Revised Statutes were discovered within the code and fixed through the amendment.

- Transportation System Plan (TSP) Update. The TSP outlines policies and strategies necessary to meet existing and future travel needs for all modes of transportation within the community.
- Complete Streets Policy. This policy is identified in the TSP as a tool the City may use to help support the construction of Complete Streets within the community.
- Vehicle Camping Pilot Program. This program was adopted by resolution in November of 2020 and included policy updates to allow vehicle camping under certain conditions within the City, in order to support organizations providing assistance to our homeless population.

Mr. Lazur clarified the Complete Streets Policy will affect future projects. The Policy is a guide to help support the construction of Complete Streets for all modes of transportation.

Commissioner Yraguen asked if eminent domain situations would be brought before the Commission.

Mr. Lazur stated, eminent domain is a last resort; however, it would not be brought before the Commission. The Hwy 138 Plan will evaluate the health of the corridor and look at strengths, existing conditions if no improvements are made and if Roseburg grows, what issues that would arise. The Plan would then recommend projects to increase the health of the corridor. The Plan may look at off-street paths for the Diamond Lake corridor, evaluate projects, and rate them based on the future outcome.

Commissioner Palmini inquired if there are other applications for townhouses, besides the Rosemary subdivision.

Mr. Lazur stated from time to time we receive applications for townhouses. The City has narrow lots and townhouses maximize the density.

Commissioner Hawks commented on the growth of apartment complexes in the Diamond Lake corridor stating she hopes retail stores will follow.

Mr. Lazur replied, there are no applications for retail stores yet; however, the hope is the Diamond Lake Urban Renewal incentives will encourage retail growth.

BUSINESS FROM COMMISSION – None

<u>ADJOURNMENT</u> - The meeting adjourned at 7:35 p.m. The next meeting is scheduled for Monday, March 1, 2021.

Chrissy Matthews

Department Technician

CITY OF ROSEBURG PLANNING COMMISSION STAFF REPORT



File No. SR-21-027 Meeting Date: April 5, 2021

To: Planning Commission

From: Stuart Cowie, Community Development Director

Subject: Site Plan Review No. SR-21-027

PROJECT SUMMARY & PROCEDURES:

Dan Zimmerman (Tait & Associates), authorized agent of the property owner and working on behalf of WinCo Foods, has submitted application for a site plan review to demolish the existing 88,000 s.f. vacant Kmart building and construct a new 72,971s.f. WinCo Foods grocery store. The development includes demolition of the existing building, reconfiguration of the parking lot, installation of new landscaping and other related site improvements. The site is an approximately 9.45 acre property zoned General Commercial (C3) that is located to the south of the intersection of Stewart Parkway and NW Mercy Drive. The property is described as Township 27 South, Range 06 West, Willamette Meridian, Section 12B, Tax Lot 1502, R62345 & Township 27 South, Range 06 West, Willamette Meridian, Section 11A, Tax Lot 0501, R62282; 2757 NW Stewart Pkwy. The property also contains the Big 5 Sporting Goods building at 2655 NW Stewart Pkwy. The applicant/owners have no plans to change the current business operations of Big 5 Sporting Goods.

Typically Site Plan Reviews, submitted to the City, are processed as a ministerial land use action in accordance with Chapter 12.10 of the Roseburg Municipal Code (RMC) and state law, which means they are processed by City staff without public notice or opportunity for appeal. Site plan review is a process applied for development review where the approval criteria, applied to the proposal, lack discretion or in other words are criteria in which the City can demonstrate that the applicant either does or does not meet the requirement by a clear metric (e.g. a property line setback, number of required off-street parking spaces). However, in unique instances larger more involved development projects can trigger certain discretionary standards within the RMC to be applied. Pursuant to RMC Chapter 12.10, when discretionary standards are applied to a land use action the action is categorized as an administrative or limited land use action and can no longer be processed as a ministerial land use action, which lacks public notice and opportunity for appeal.

The City held a pre-application conference with the applicant in November of 2020 and after several discussions with the applicant it was determined the request would contain a number of discretionary approval criteria. Therefore, pursuant to Section 12.06.010(C) and Section 12.10.010(B) the application is being processed as a limited land use decision and referred directly to Planning Commission based on the request of the applicant.

APPLICABLE CRITERIA:

The applicant's request for a Site Plan Review application was reviewed by the City based on the applicable criteria as follows from the Roseburg Municipal Code:

- RMC Section 12.04.040 "Commercial Districts"
- RMC Section 12.06 "Site Development"
- RMC Section 12.08.040(F) "Clear Vision Areas"

CRITERIA ANALYSIS:

- 1. Chapter 12.04.040 "Commercial Districts"
 - a. The applicant's proposal for a grocery store is permitted within the C3 zone pursuant to Section 12.04.040(B) – Table 2-7 "Commercial – Allowed Uses" – Use number 46 – "Grocery Store..."
 - b. The applicant's site plan (Exhibit A) has been reviewed in relation to and are compliant with the applicable standards contained within Section 12.04.040(B) Table 2-8 "Commercial Parcel and Building Standards".
- 2. Chapter 12.06 "Site Development"
 - a. Section 12.06.010(D)(2)
 Compatibility. Compatibility with the surroundings and the Comprehensive Plan's designation for uses on surrounding property, particularly when the surrounding property is residential in character. In applying this standard and criteria:
 - i. Odor, dust, smoke, fumes, noise, glare, heat, and vibration from uses which might create a nuisance or be offensive to other uses in the area or be incompatible with such other uses, shall be adequately eliminated or controlled.
 - 1. The proposed use for the site is compatible with the surrounding area uses as described in the 2019 Oregon Structural Specialty code, Chapter 3 Occupancy Classification and Use, as described below. The proposed site was formerly occupied by Kmart with a mercantile (M) occupancy and is the same occupancy proposed by WinCo Foods. The surrounding property is not residential in character. Adjacent uses include Albertsons (M occupancy) to the east, Big 5 and Sherm's Thunderbird Market (both M occupancies) to the west, and Mercy Medical Center (I-2/B Occupancy) to the north.
 - 2. The proposed design and operations of the Win Co Foods store will not create a nuisance or be offensive to neighboring properties. Win Co Foods store will operate in the same fashion as the neighboring Albertson's and Sherm's Thunderbird grocery stores. The location of the truck delivery area and trash compactors is designed to face away from public streets. A dumpster or dumpster enclosures is not proposed due to the trash compactors. Ventilation for HVAC systems will be located on the roof. Materials have been chosen to not create glare or vibration to adjacent properties as shown in the submittal

^{**}Note: The sections listed above are not reviewed in their entirety, as only portions of the sections are applicable to the applicant's request.

package documents. It is anticipated that Win Co Foods will generate no more odor, noise or vibration than what is typical of an average grocery store, the neighboring grocery stores Albertson's and Sherm's Thunderbird, and other surrounding uses, all of which are allowed within the C3 zone.

- ii. Due consideration shall be given to the preservation of historical and natural features.
 - 1. This section does not apply to this submittal as no historical elements are located on the site and no existing natural features will be disturbed.
- iii. Nonconforming uses shall not take precedence over a proposed development which enhances the aesthetics or value of the surrounding property.
 - The current zoning designation C3 General Commercial and standards within the municipal code require the proposed WinCo Foods grocery store to be in conformance thus deeming this section not applicable.
- iv. This standard and criteria shall not take precedence over the need for housing for all income groups in the City.
 - 1. This section does not apply to the WinCo Foods submittal as housing is not proposed in this development.
- v. Signs shall be of a scale that is in harmony with the site and surrounding development and may be illuminated if within the lighting and other standards and criteria of this Section.
 - Building and free-standing signage is not proposed at this time and will be a separate submittal meeting the requirements above during the building plan review submittal.
- b. Section 12.06.020 "Public Improvement Requirements"
 - i. The applicant in coordination with the City has modified their plans to include increased spacing of the existing access points, limiting turn movements to right-in and right-out for the eastern access and including a "no turn on red" traffic sign for the right turn movement at the signalized intersection of Stewart Pkwy and Mercy Drive. In addition, the applicant previously met with City Staff to develop a scope for a traffic impact study, which was coordinated with and reviewed by the City to ensure that it adequately addressed the criteria contained within Section 12.06.020 of the RMC. Subsequently during the submission of the application, the applicant provided a TIS, dated January 13, 2021 (Exhibit C). After review, City staff has concluded that the TIS follows the agreed upon scope and adequately addresses the applicable criteria of Section 12.06.020 of the RMC.
- c. Section 12.06.030 "Site Improvement Requirements"
 - i. City staff has reviewed the applicant's site plan (Exhibit A) and civil plans (Exhibit D), which adequately demonstrate compliance with the applicable portions of the following sections of Chapter 12.06.030:
 - Section 12.06.030(D) "Underground Utilities"
 - 2. Section 12.06.030(E) "Lighting"
 - 3. Section 12.06.030(F) "Screening"
 - 4. Section 12.06.030(G) "Garbage Container areas"
 - 5. Section 12.06.030(H) "Off-Street Parking"
 - 6. Section 12.06.030(I) "Bicycle Parking Standards"
 - 7. Section 12.06.030(J) "ADA Parking Standards"
 - 8. Section 12.06.030(K) "Parking Stall Standards"
 - 9. Section 12.06.030(N) "Off-Street Loading"
 - 10. Section 12.06.030(Q) "Parking Area and Driveway Design"

- 11. Section 12.06.030(S) "Parking Area Improvements"
- 12. Section 12.06.030(T) "Landscaping and Buffering"
- 13. Section 12.06.030(W) "Internal Walkways"
- ii. The applicant has submitted a storm water report (Exhibit E), dated January 15, 2021, and included the necessary storm water improvements/design within the civil plans set. Upon review of the civil plans and the storm water report, the City has determined that the applicant is in compliance with the requirements of Section 12.06.030(C) "Surface Water Drainage".
- iii. Additionally, the applicant, as contained on page 3 of the "Development Narrative Letter" (Exhibit B) has provided supplemental information in regards to the internal pedestrian walkways, addressing Section 12.06.030(W) of the RMC. More specifically, the two locational criteria taken from Section 12.06.030(W)(3), state:
 - 1. Each property subject to the provisions of this Section shall provide pedestrian walks at or around the building of sufficient extent to provide safe and convenient pedestrian passage commensurate with the character of the development and the nature of the intended use of the building.
 - a. The proposed design demonstrates pedestrian connectivity from the parking lot, main circulation to and from the building, and to the public way along Stewart Parkway in two locations. Planning pedestrian circulation to the north and west fits within the character of the development while providing separation of the public and store back-of-house operations.
 - 2. Each such property shall provide pedestrian walkways, crosswalks and other pedestrian facilities to allow safe and convenient pedestrian access throughout the site, particularly to routes between main building entrances and adjacent pedestrian destinations, including uses on adjoining properties, public sidewalks, and transit stops. Where the nature of the development is such that several buildings utilize a common internal pedestrian walkway system, such internal walkway system shall extend to the street sidewalk and shall serve to meet the requirements of this Section.
 - a. The updated proposed site plan provides an additional pedestrian pathway from the center parking aisle north to the signalized intersection at Mercy and Stewart Parkway and south to the property line for adjacent property connectivity. This proposed pedestrian accessway along with the pedestrian pathway from the front of the building to Stewart Parkway provides multiple accesses to public sidewalks, building entrances, and adjacent properties. All pedestrian accessways are at grade, constructed with concrete for a contrasting material, and are marked with striping when crossing vehicular traffic. Speedbumps have also been shown at the front main drive aisle for additional safety. The pedestrian walkways identified within the plans have been designed in such a way as to allow safe and convenient pedestrian access throughout the site.
- 3. Chapter 12.08.040 "Supplemental Provisions"
 - a. Section 12.08.040(F) "Clear Vision Areas"
 - i. The applicant's plans including all landscaping plantings have been reviewed for compliance with the clear vision standards of the aforementioned section. Staff has no concerns regarding any impeded clear vision areas. It is notable that during the preapplication conference and scoping of the TIS, the City raised concerns regarding a

limited site distance looking west for vehicles exiting by right turn (eastbound) from the signalized intersection. This is based on pre-existing conditions of the retaining wall along the Big 5 Sporting Goods building Stewart Parkway frontage. While there is no reasonable physical alternative to improve the impeded site distance, the applicant has addressed the issue within the TIS by identifying a modification to the existing signalized intersection to limit eastbound exiting traffic to a "no-turn on red" turning movement.

STAFF RECOMMENDATION:

Staff determines that the Site Plan Review request, as submitted by the applicant, satisfies the criteria for approval. Therefore, it is recommended the Planning Commission direct the applicant to prepare findings of fact approving the Site Plan Review as contained within file SR-21-027 for review and adoption during the May 3, 2021 Planning Commission meeting.

OPTIONS:

- Direct the applicant to prepare findings approving the Site Plan Review request.
- Continue consideration of the request.
- Direct staff to prepare findings denying the Site Plan Review request.

SUGGESTED MOTION:

I MOVE TO DIRECT THE APPLICANT TO PREPARE FINDINGS OF FACT APPROVING THE SITE PLAN REVIEW, AS REQUESTED AND PRESENTED WITHIN FILE SR-21-027, FOR REVIEW AND ADOPTION DURING THE MAY 3, 2021 PLANNING COMMISSION MEETING.

EXHIBITS:

Exhibit A – Applicant's Site Plan

Exhibit B – Development Narrative Letter

Exhibit C – Traffic Impact Study (Pages 1 – 34)

Exhibit D – Civil Plans

Exhibit E – Storm water Technical Memo (Pages 1 – 8)

SITE DATA

SITE STANDARDS (TITLE 12.06 SITE DEVELOPMENT): APN: R62345

ZONING: C3 - GENERAL COMMERCIAL

TOTAL SITE AREA: 412,548 S.F. / 9.47 ACRES

FAR: 20% INCLUDES BOTH STRUCTURES WINCO BUILDING AREA: 72,971 S.F.

BIG 5 BUILDING AREA: 9,790 S.F.

TOTAL BUILDING AREA: B2,761 / 412,548 = 20%

SETBACKS (12.04.040 TABLE 2-8):

FRONT: 0

REAR: 0 SIDES: 0

MAX ALLOWABLE BUILDING HEIGHT (12.04.040 TABLE 2-8): 80'-0"

MAX. WINCO BUILDING HEIGHT PROVIDED: 34'-10"

OCCUPANCY: M - MERCANTILE

CONSTRUCTION TYPE: VB FULLY SPRINKLERED

60' NO BUILD EASMENT

LANDSCAPING STANDARDS (12.06.030):

TOTAL PARKING AREA: 136,500 x .05 = 6,825 S.F.

TOTAL PARKING LANDSCAPE REQUIRED: 5% (6,825 S.F.)

TOTAL PARKING LANDSCAPE PROVIDED: 5.2% (7,132 S.F.)

PARKING STANDARDS (12.06.030 TABLE 3-3):

WINCO (NEW WORK)

TOTAL PARKING AREA: 120,750 S.F.

PARKING STALLS REQUIRED: 246

73,800/300 = 246

PARKING STALLS PROVIDED: 342

PARKING STALL SIZE: 9'-6"x20'

ADA STALLS REQUIRED (TABLE 3-4): 8

ADA STALLS PROVIDED: 8

BICYCLE SPACES REQUIRED: 17

1 PER 15 REQUIRED AUTO SPACES

246/15 = 16.4

BICYCLE SPACES PROVIDED: 28

BIG 5 (EXISTING TO REMAIN)

TOTAL BIG 5 PARKING AREA: 15,750 S.F.

PARKING STALLS REQUIRED: 33

9,790/300 = 32.6

PARKING STALLS PROVIDED: 45

PARKING STALL SIZE: 9'x18'

BICYCLE SPACES REQUIRED: 3

1 PER 15 REQUIRED AUTO SPACES

33/15 = 2.2

BICYCLE SPACES PROVIDED: 3

CONTACT INFORMATION

WINCO FOODS, LLC 650 N. ARMSTRONG PLACE BOISE, ID 83704 PHONE: 208-377-0110 CONTACT: GREG GOINS E-MAIL: greg.goins@wincofoods.com

LAND USE COUNCIL: MILLER NASH GRAHAM & DUNN, LLP 3400 U.S. BANCORP TOWER 111 S.W. FIFTH AVENUE PORTLAND, OREGON 97204 PHONE: 503-224-0155

CONTACT: KELLY HOSSAINI, P.C. EMAIL: kelly.hossaini@millernash.com

CIVIL: SCJ ALLIANCE 212 N TOWER AVENUE CENTRALIA. WA 98531 PHONE: 360-352-1465

CONTACT: BRANDON JOHNSON, PE E-MAIL: brandon.johnson@scjalliance.com

APPLICANT/ARCHITECT: TAIT & ASSOCIATES, INC 707 NORTH 27TH STREET BOISE, IDAHO 83702

PHONE: 1-208-319-0772 CONTACT: DANIEL ZIMMERMAN

EMAIL: dzimmerman@tait.com

VICINITY MAP

LEGEND

#

TOTAL

NUMBER OF STANDARD

NUMBER OF PARKING SPACES

NEW CONCRETE PEDESTRIAN ACCESS

PARKING SPACES

BICYCLE PARKING

PROPERTY/PARCEL LINE

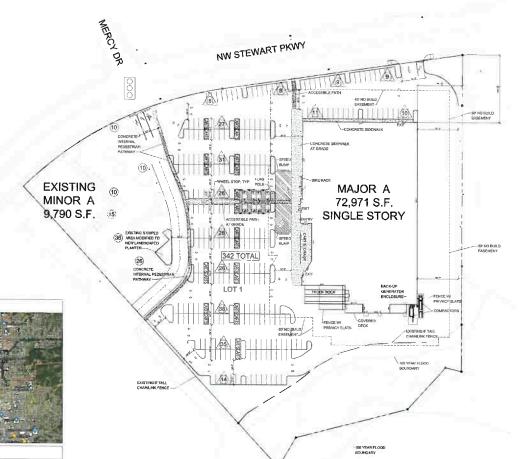
60' NO BUILD EASMENT LINE

CART CORRAL

PER PARCEL

EXHIBIT # A

DATE 11/20/20 12/04/20 12/08/20 1/12/21 1/27/21 2/03/21 2/22/21



SITE PLAN

SHEET TITLE

WINCO FOODS STORE
NW STEWART PRKWY &
MERCY DR
ROSEBURG, OREGON

PRELIMINARY SITE PLAN

SHEET C-1





707 N. 271H Street, Boise, ID 83702

p:208/319/0772 www.tait.com

Development Narrative Letter

February 22, 2021

City of Roseburg Richard Hoffman Community Development 900 SE Douglas Avenue Roseburg, OR 97470 (541) 492-6874

RE: Development Narrative Letter WinCo Foods Located at the SEC of NW Stewart Parkway & Mercy Drive, Roseburg, Oregon

Summary

WinCo Foods proposes in this submittal to demolish the existing 88,000s.f. vacant Kmart building, site parking field, and construct a new 72,971s.f. WinCo Foods grocery store located at the Southeast corner of NW Stewart Parkway and Mercy Drive in Roseburg, Oregon (Douglas Co.) and perform necessary improvements to construct and operate the proposed WinCo Foods store. The assessor's parcel number is R62345.

Legal Description:

Situated in the northeast quarter of section 11 and the northwest quarter of section 12, township 27 south, range 6 west, Willamette Meridian, City of Roseburg, Douglas County, Oregon

Current zoning is C-3: General Commercial and allows for a WinCo Foods store as a use permitted outright. The proposed WinCo Foods will be a 72,971 square foot, single story building with operation hours of 24 hours a day 7 days a week. The maximum building height of the proposed WinCo Foods store is 34'-10" above finish floor with the highest point located at the main entry/exit.

The site is within a heavily commercial area, with grocery stores directly to the east and west, and Mercy Medical Center across Stewart Parkway. To the south of the site are additional medical office buildings. Also nearby is a Walmart Supercenter and a Goodwill store. The transportation system in the area is ideally suited for a grocery store, as Stewart Parkway is a five-lane major arterial, which feeds into Edenbower Boulevard (a minor arterial) to the east and Garden Valley Boulevard (a major arterial) to the south.

Compliance with Site Plan Review Criteria (Section 12.06.010.D.2.a through e)

Section D.2: Compatibility

Compatibility with the surroundings and the Comprehensive Plan's designation for uses on surrounding property, particularly when the surrounding property is residential in character. In applying this standard and criteria:

- The proposed site is consistent with the compatibility of both the existing use and surrounding area uses as described in the 2019 Oregon Structural Specialty code, Chapter 3 Occupancy Classification and Use, and described below.
- The proposed site was formerly occupied by Kmart with a mercantile (M) occupancy is the same occupancy proposed by WinCo Foods.
- As noted above, the surrounding property is not residential in character. Adjacent uses include Albertsons (M occupancy) to the east, Big 5 and Sherm's Thunderbird Market (both M occupancies) to the west, and Mercy Medical Center (I-2/B Occupancy) to the north.
- a) Odor, dust, smoke, fumes, noise, glare, heat, and vibration from uses which might create a nuisance or be offensive to other uses in the area or be incompatible with such other uses, shall be adequately eliminated or controlled.
 - The proposed design and operations of a WinCo Foods store limit the above criteria as to not create a nuisance or be offensive to neighboring properties with the location of the truck delivery area and trash compactors facing away from public streets, no dumpsters or dumpster enclosures in use due to trash compactors, and exhaust ventilation for service departments located on the roof. Materials have been chosen to not create glare or vibration to adjacent properties as shown in the submittal package documents.
- b) Due consideration shall be given to the preservation of historical and natural features.
 - This section does not apply to this submittal as no historical elements are located on the site and no existing natural features will be disturbed.
- c) Nonconforming uses shall not take precedence over a proposed development which enhances the aesthetics or value of the surrounding property.
 - The current zoning designation C-3 General Commercial and standards within the municipal code require the proposed WinCo Foods grocery store to be in conformance thus deeming this section not applicable.
- d) This standard and criteria shall not take precedence over the need for housing for all income groups in the City.
 - This section does not apply to the WinCo Foods submittal as housing is not proposed in this development.

- e) Signs shall be of a scale that is in harmony with the site and surrounding development and may be illuminated if within the lighting and other standards and criteria of this Section.
 - Building and free-standing signage is not proposed at this time and will be a separate submittal meeting the requirements above during the building plan review submittal.

Compliance with Site Plan Review Criteria (Section 12.06.030.W.3)

Section W.3: Internal Walkways

Locations. Each property in all Zoning Districts being newly developed or redeveloped in accordance with Subsection 12.06.010(D) shall provide internal sidewalks and walkways, subject to the requirements and exceptions set forth in this section.

- a) Each property subject to the provisions of this Section shall provide pedestrian walks at or around the building of sufficient extent to provide safe and convenient pedestrian passage commensurate with the character of the development and the nature of the intended use of the building.
 - The proposed design demonstrates pedestrian connectivity from the parking field, main circulation to and from the building, and to the public way along Stewart Parkway in two locations. Planning pedestrian circulation to the north and west fits within the character of the development while providing separation of the public and store back-of-house operations.
- b) Each such property shall provide pedestrian walkways, crosswalks, and other pedestrian facilities to allow safe and convenient pedestrian access throughout the site, particularly to routes between main building entrances and adjacent pedestrian destination, including uses on adjoining properties, public sidewalks, and transit stops. Where the nature of the development is such that several buildings utilize a common internal pedestrian walkway system, such internal walkway system shall extend to the street sidewalk and shall serve to meet the requirements of this section.
 - The updated proposed site plan provides an additional pedestrian pathway from the center parking aisle north to the signalized intersection at Mercy and Stewart Parkway and south to the property line for adjacent property connectivity. This proposed pedestrian accessway along with the pedestrian pathway from the front of the building to Stewart Parkway provides multiple accesses to public sidewalks, building entrances, and adjacent properties.
 - All pedestrian accessways are at grade, constructed with concrete for a
 contrasting material, and are marked with striping when crossing vehicular
 traffic. Speedbumps have also been shown at the front main drive aisle for
 additional safety.

The ends of all raised walkways, where the walkway intersects a drive, parking aisle or street shall provide ADA accessible ramps.

We look forward to working with the city of Roseburg on this Site Plan Review to redevelop the proposed site; not only to provide a service to the community, but also to bring revitalization to a vacant site.

Should you have any questions or need any additional information, please do not hesitate to contact me.

Sincerely,

Daniel Zimmerman | TAIT & Associates, Inc.

707 N. 27th Street, Boise, ID 83702

Ph: 208-319-0772



Traffic Impact Analysis

WinCo Foods Roseburg, OR

Prepared For:

WinCo Foods

Prepared By:

SCJ Alliance Anne Sylvester, PTE 8730 Tallon Lane NE, Suite 200 Lacey, WA 98516 360.352.1465

January 2021



Traffic Impact Analysis – WinCo Foods

WinCo Foods, Roseburg

Project Information

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Signature

The technical material and data contained in this document were prepared under the supervision and direction of the undersigned, whose seal, as a professional engineer licensed to practice as such, is affixed below.



anne Sylvester

Prepared by Anne Sylvester, PTE and Ryan Shea, PTP

Approved by Eric Johnston, PE

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1 INTRODUCTION

1.1 Project Overview

WinCo Foods is proposing to rebuild an existing apppromximate 85,000-square foot vacant commercial building into a WinCo Foods store in Roseburg, Oregon. The proposed WinCo Foods store will be approximately 74,000-square feet in size and will be located on NW Stewart Parkway to the west of the intersection with Edenbower Boulevard and opposite the Mercy Hospital and Medical Center.

Figure 1 illustrates the site vicinity and the transportation network serving the project area. **Figure 2** presents the proposed project site plan.

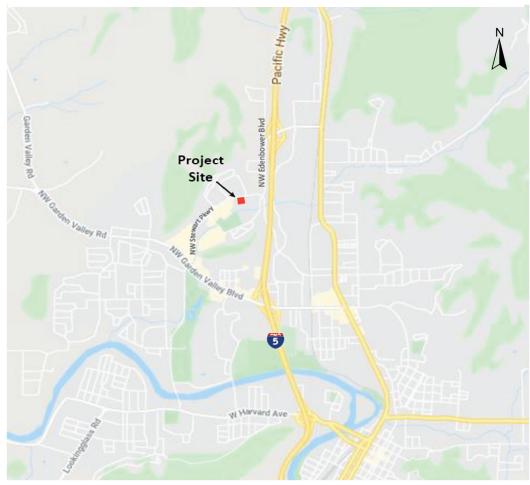
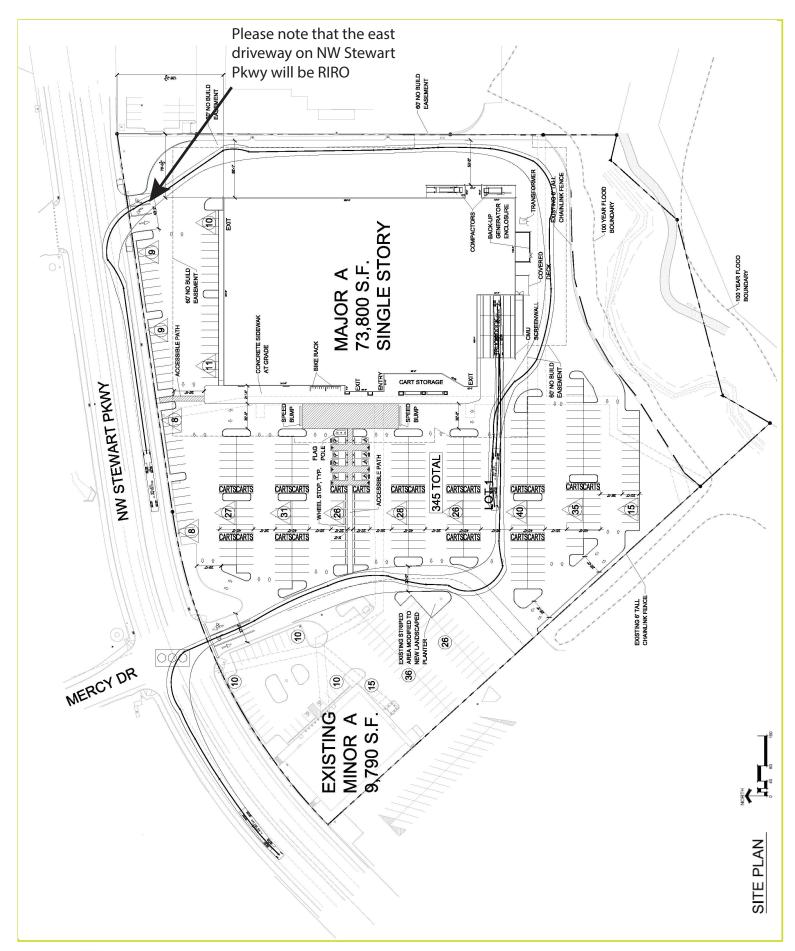


Figure 1. Site Vicinity Map

1.2 Project Description

The proposed project will consist of rebuilding an approximate 85,000 square foot vacant building, previously occupied by Kmart, to accommodate an approximate 74,000 square foot WinCo Foods Store. As part of the renovation a portion of the existing structure nearest to NW Stewart Parkway will be removed to provide additional parking north of the building. Access to the project will be provided by





WinCo Foods Roseburg, Oregon Traffic Impact Study

two existing driveways along NW Stewart Parkway. One of these driveways connects to the existing traffic signal-controlled intersection with Mercy Drive and will provide for all turning movements. The other driveway is currently located near the west edge of the vacant building and will be located to the east edge of the rebuilt WinCo Foods store. It will be limited to right in/right out movements. The project is anticipated to open in 2022.

1.3 Study Context

A Traffic Scoping Analysis was prepared and submitted to the City of Roseburg on November 11, 2020. The scoping analysis identified potential project trip-making characteristics including trip generation and assignment, and suggested parameters for analysis of project-related traffic impacts.

This report has been prepared to provide the traffic analysis and project information necessary for project review by the City. It has been prepared in accordance with City standards and specific guidance received through a project conference and follow-up emails and conversations. The analysis covered in this report focuses on existing and forecasted operations of the following intersections:

- NW Stewart Parkway at NW Edenbower Blvd
- NW Stewart Parkway at W Mercy Drive/Site Driveway
- NW Stewart Parkway at Renann Street
- NW Stewart Parkway at NW Garden Valley Blvd

Operational analysis has been prepared for existing 2020 conditions, forecasted operations in the 2022 project opening year, and operations in five year increments out to the long-term horizon year of 2042. This report also discusses multimodal transportation facilities and services and transportation safety.

1.4 Report Content and Organization

This report has been organized into six chapters, the first of which is this Introduction. Chapter 2 presents a discussion of existing land use and transportation conditions in the study area including physical characteristics of the roadway, existing traffic volumes, crash data and analysis, transit service and bicycle and pedestrian facilities.

Chapter 3 discusses the characteristics of the proposed WinCo Foods project and provides an estimate of existing PM peak hour project-related trip generation and distribution.

Chapter 4 highlights planned transportation system improvements that could affect the immediate study area, as well as the forecasts for future year PM peak hour traffic including the 2022 expected year of project opening through the long-term planning horizon year of 2042 with data for five year increments in between. Projections were prepared for conditions both with and without the proposed project for each five year time period.

Chapter 5 includes a discussion of traffic analysis methodology and assumptions, along with the operational analysis results for the PM peak hour in 2020, 2022, 2027, 2032, 2037 and 2042.

Chapter 6 presents a summary of key findings, conclusions, and recommendations with respect to expected traffic impacts.

2 EXISTING CONDITIONS

2.1 Area Land Uses

The WinCo Foods will be located on the south side of NW Stewart Parkway between the east and west legs of Mercy Drive and opposite the Mercy Medical Center. The project will be located in an existing, rebuilt structure that formerly housed a K-Mart store. Access to the facility is currently provided via the south leg of the NW Stewart Parkway/E Mercy Drive intersection which is signalized and via the stop-controlled driveway approximately 300 feet to the east of this intersection. In addition to the Mercy Medical Center, adjacent land uses include a mix of commercial, congregate residential (retirement village) and medical offices.

2.2 Roadway Inventory

2.2.1 NW Stewart Parkway

NW Stewart Parkway is classified by the City of Roseburg as a major arterial and has a posted speed of 35 mph. Within the study area NW Stewart Parkway is a five-lane roadway, with two travel lanes in each direction and a two-way center turn lane with left and/or right turn channelization at key intersections. Bike lanes and sidewalks are provided on both sides of the road.

2.2.2 Edenbower Boulevard

Edenbower Boulevard is classified by the City of Roseburg as a minor arterial. North of NW Stewart Parkway this street is a three-lane roadway with one travel lane in each direction and a continuous two-way center turn lane. Additional channelization is provided for left turns at the intersection with NW Stewart Parkway. This portion of Edenbower Boulevard is posted for 25 mph speeds and has bike lanes and sidewalks along both sides of the road. South of NW Stewart Parkway, Edenbower Boulevard is generally also a three-lane roadway with a single travel lane in each direction and a continuous left turn lane. Left and right turn channelization is provided at the intersection with NW Stewart Parkway. This portion of the road is posted for 40 mph speed with bike lanes and sidewalks on both sides.

2.2.3 Mercy Drive

Mercy Drive is classified by the City of Roseburg as a local street, with a posted speed of 15 mph. Within the study area Mercy Drive is a two-lane roadway with sidewalks along portions of the street and no bike lanes. The street is a loop road with another intersection approximately 700 feet to the east on NW Stewart Parkway and largely serves the Mercy Medical Center and adjacent medical buildings.

2.2.4 Renann Street

Renann Street is classified by the City of Roseburg as a minor collector street with bike lanes and sidewalks on both sides. Within the study area this street is a three-lane roadway with a continuous two-way left turn lane.

2.2.5 Garden Valley Boulevard

Garden Valley Boulevard is classified by the City of Roseburg as a major arterial. Within the study area Garden Valley Boulevard is a five-lane roadway, with two travel lanes in each direction and a two-way center left turn lane. Additionally, lane channelization for lefts and westbound rights is provided at the

intersection with NW Stewart Parkway. Sidewalks and bike lanes are provided on both sides of the road. Within the project vicinity Green Valley Boulevard has a posted speed of 35.

A summary of the existing intersection channelization and control type for each of the study area intersections is provided in **Figure 3.**

2.3 Existing Traffic Conditions

Traffic Count Consultants, Inc. (TC2), a transportation data collection service, provided evening turning movement counts for the study area intersections. The counts were conducted between 4:00 pm and 6:00 pm on October 27th and 28th, 2020 at the following locations:

- NW Stewart Parkway at NW Edenbower Boulevard
- NW Stewart Parkway at Mercy Drive/Site Driveway
- NW Stewart Parkway at Renann Street
- NW Stewart Parkway at NW Garden Valley Boulevard

Turning movement counts are included in **Appendix A**.

2.3.1 Seasonal Adjustment Factor

Per guidance provided in the ODOT *Analysis Procedures Manual* existing PM peak hour traffic counts were adjusted to create seasonally-adjusted design hourly volumes that account for the variations in traffic volumes over the course of a year. The design hour volume is the 30th highest hour volumes for a given year and are used to "normalize" traffic count data to evaluate the impacts of land development projects and guide the analysis of roadway and intersection improvement needs.

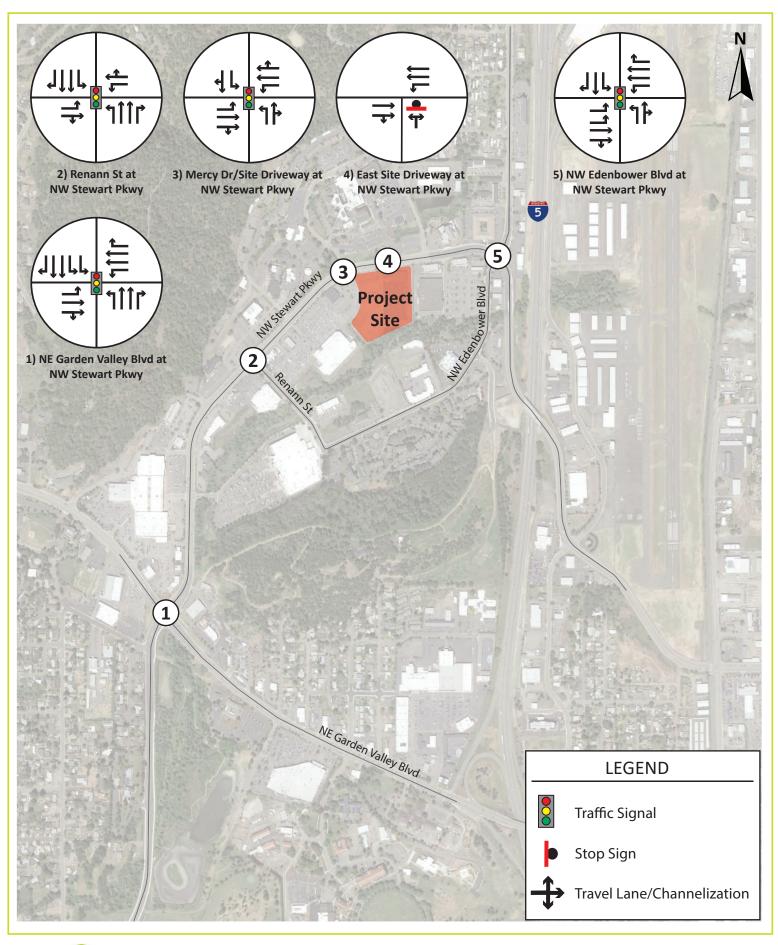
To calculate the appropriate seasonal adjustment factor for intersections along NW Stewart Parkway, monthly traffic data at Automatic Traffic Recording Station (ATR 10-006) on I-5 to the north of Roseburg was used. Based on this data, July is the peak travel month in the area. Since the counts were taken in October, an adjustment factor of 1.1065 was applied to all turning movements at study area intersections. The calculation is included with turning movement count diagrams in **Appendix A**. The adjusted volumes are presented in **Appendix C**.

2.3.2 COVID-19 Adjustment

Based on input from City of Roseburg staff, no traffic volume adjustment was made to reflect impacts attributable to COVID-19. According to city staff, traffic levels during the count period were not considered to be substantively different from data counted during that time period in previous years. Additionally, research was conducted into the ODOT COVID adjustment procedures and no locations or comparable roadway types upon which to base an adjustment were identified.

2.3.3 Adjusted Traffic Data

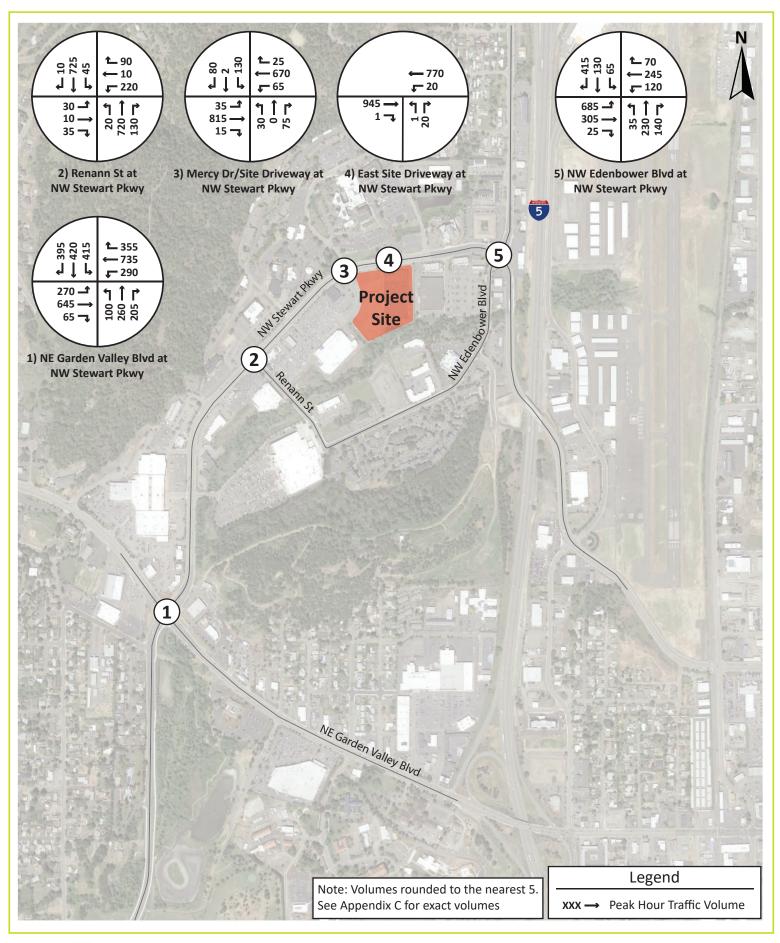
The traffic count data included heavy vehicles and bicycles for each intersection approach, and pedestrian counts at each intersection crossing. The existing adjusted 2020 traffic PM peak hour volumes for study area intersections are presented in **Figure 4**. Adjusted traffic volumes are provided in **Appendix A**.





WinCo Foods Roseburg, Oregon Traffic Impact Study

Figure 3 Intersection Channelization and Control





WinCo Foods Roseburg, Oregon Traffic Impact Study

Figure 4
Existing 2020 PM Peak Hour
Traffic Volumes

2.4 Crash History

Crash data was obtained from the Oregon Department of Transportation Crash Analysis and Reporting Unit for the study area intersections. The data was collected over the five-year span between January 1, 2015 and December 31, 2019 and reviewed for crash occurrences at the study area intersections.

There were six serious injury crashes reported and no fatal crashes during the five-year period reviewed. Overall, approximately 83 percent of all the reported crashes within the study area were classified as property damage only (no apparent injury) or potential injury (complaint of pain). The total crashes by severity are provided in **Table 1**.

Crash rates were calculated for each intersection using average daily volumes and the number of collisions reported per intersection. Generally crash rates above one crash per million entering vehicles (CMEV) are considered a possible safety concern and warrant further analysis and possible mitigation. There are no intersections in the study area showing a crash rate above one. The crash rates are provided in **Table 1.** The crash data is included in **Appendix B.**

	Crash Type								Crash Severity		
Intersection	Rear End	Side- swipe	Turns	Angle	Peds	Other	Total	Volumes	Crash Rate	Injury	PDO
NW Stewart Pkwy/Edenbower Blvd	16	8	7	2	1	1	35	24,630	0.779	19	16
NW Stewart Pky/W Mercy Drive	2	0	23	1	0	1	27	19,400	0.763	12	15
NW Stewart Pky/ Renann Street	3	1	12	5	0	0	21	20,450	0.563	13	8
NW Stewart Pky/ Garden Valley Blvd	30	6	11	1	1	0	49	41,630	0.645	26	23

Table 1. Intersection Crash Data Analysis

Note: Other crashes include head-ons, fixed objects and other miscellaneous incidents. Crash rates are expressed in terms of rates per Million Entering Vehicles or MEV. PDO means Property Damage Only

Within the study area two intersections were reported with serious injury crashes - the intersections of NW Stewart Parkway with Garden Valley Boulevard and Renann Street. These crashes are described below by intersection.

2.4.1 NW Stewart Parkway and Garden Valley Boulevard

The intersection of NW Stewart Parkway and Garden Valley Boulevard has experienced the highest number of crashes in the study area, with approximately 10 crashes per year. The most common crash type identified was rear-end, and 53 percent of these crashes were identified as resulting in an injury or potential injury. Twenty-six injury crashes were reported at this intersection. Of these only two resulted in a serious injury, the remaining twenty-four crashes were identified as potential injury (73 percent) or minor injury (19 percent). The serious injury crashes are described below.

A serious injury crash was reported at 8:00 pm on December 11, 2019. It involved three
passenger vehicles. One vehicle was traveling southbound making a left at the signal, the other
two vehicles were traveling northbound. The crash cause was reported as disregarded traffic
signal.

2. A serious injury crash was reported at 1:00 pm on December 23, 2016. It involved two passenger vehicles both traveling northbound. The crash type was identified as rear-end and the crash cause was reported as following too closely.

2.4.2 NW Stewart Parkway and Renann Street

The intersection of NW Stewart Parkway and Renann Street has experienced the lowest number of crashes in the study area, with approximately four crashes per year. The most common crash type involved turns with 62 percent of these crashes resulting in an injury or potential injury. Thirteen injury crashes were reported at this intersection with four resulting in a serious injury. The serious injury crashes are described below.

- 1. A serious injury crash was reported at 8:00 am on April 6, 2018. It involved four passenger vehicles. One vehicle was traveling northbound, the other three vehicles were traveling southbound making left turns. The crash cause was reported as one vehicle turning left who made an improper turn and did not yield right-of-way.
- 2. A serious injury crash was reported at 4:00 pm on May 11, 2019. It involved five passenger vehicles. Two vehicles were traveling northbound, the other three vehicles were traveling southbound making a left turn. The crash cause was reported as one vehicle turning left made an improper turn and did not yield right-of-way.
- 3. A serious injury crash was reported at 1:00 pm on July 25, 2019. It involved one motorcycle and one passenger vehicle. The motorcycle was traveling southbound and the vehicle was traveling northbound making a left turn. The crash cause was reported as the vehicle making an improper turn and not yielding right-of-way.
- 4. A serious injury crash was reported at 8:00 am on September 25, 2019. It involved three passenger vehicles. One vehicle was traveling northbound, the other two vehicles were traveling westbound making left turns. The crash cause was reported as the vehicle traveling northbound disregarding the traffic signal.

2.5 Transit

Public transportation service is provided to Roseburg and other destinations in Douglas County by Umpqua Transit (UTran). Fixed route service in the immediate project area is provided by:

- The **Greenline** which connects the area to Umpqua Community College on the north and downtown Roseburg and the Garden Valley Boulevard, Harvard Avenue, and Diamond Lake Boulevard corridors on the south. Services operate Monday through Friday on an hourly basis between the hours of 6:35 am and 8:10 pm.
- The Redline which connects the area to Umpqua Community College on the north and downtown Roseburg and the Harvard Avenue corridor on the south.

Full fare is \$2.00 per ride with discounts for special needs and seniors. In addition, a UCC pass is available to all UCC students. The pass must be purchased at the UCC Administration office and is good for one term of college only. All buses are ADA accessible with wheelchair lifts. In addition, all Umpqua Transit buses are equipped with bike racks to extend the area of service coverage beyond normal walking distances to/from bus stops.

In addition to fixed route service, Umpqua Transit offers a Direct Paratransit program that is a share-ride service for people with disabilities that prevent them from using regular service. UTrans Direct service operates during the same hours and service is available within ¾ of an air mile of the UTrans fixed route service in Roseburg.

2.6 Bicycle and Pedestrian Facilities

As indicated in the foregoing discussion of each study area roadway, bicycle and pedestrian facilities are provided along most roadway segments, with the exception of portions of Mercy Drive. Existing bicycle and pedestrian facilities are available to serve these modes of travel to and from the proposed *WinCo Foods* site.

3 PROJECT TRAFFIC CHARACTERISTICS

The project-related characteristics having the most effect on area traffic conditions are peak hour trip generation and the directional distribution of traffic volumes on the surrounding roadway network.

3.1 Site-Generated Traffic Volumes

Vehicle trip generation was estimated using the rates contained in the 10th edition of the <u>Trip</u> <u>Generation Manual</u> published by the *Institute of Transportation Engineers (ITE)*. For the proposed WinCo Foods, the land-use category "Discount Supermarket" (land-use code 854) was used. For this analysis, the "fitted-curve" equation was used to estimate trips in preference to using a specific trip rate as this approach was recommended by ITE.

A trip generation calculation was also performed for the previous tenant, Kmart, using the land-use category "Free-Standing Discount Store" (Land-use code 815). These trips are subtracted from the proposed project trips to produce a total trip increase with the WinCo project. Land use code 815 was chosen for this analysis as being the most representative of a Kmart facility, which had a centralized cashier area (unlike the decentralized approach used in department stores) and it sold products at a discount.

Trip rates reflect the PM peak hour of the adjacent street system when traffic is typically the heaviest, rather than peak hour of the generator.

Pass-By Trips

It is anticipated that this project will attract some traffic from people already driving on area roadways. These trips are not new trips added to the local roadways (primary trips) but represent "pass-by" trips according to the following definition:

Pass-by trips: Pass-by trips are trips made as an intermediate stop from an origin to a primary destination (i.e., stopping to shop on the way home from work) by vehicles passing directly by the project driveway.

Based on the 3rd Edition of the ITE <u>Trip Generation Handbook</u>, the pass-by percentage for Discount Supermarket is 21 percent and for Free-Standing Discount Store is 17 percent. The pass-by trips were assigned to the NW Stewart Parkway at Mercy Dr/West Site Driveway, taken equally from both travel directions on NW Stewart Parkway.

The trip generation rates used for the PM peak hour are shown in Table 2.

Table 2. PM Peak Hour Trip Generation Characteristics

Land Use	Land Use Code (LUC)	Unit	Trip Rate	Pass-By %	Enter %	Exit %
Discount Supermarket	854	1,000 sq ft	8.30	21%	50%	50%
Free-Standing Discount Store	815	1,000 sq ft	4.83	17%	50%	50%

The total trip generation expected from this project is calculated by applying the unit measure for each land use category to the appropriate trip generation rate. The PM peak hour trip generation calculations for the WinCo project and the previous Kmart store are shown in **Table 3.**

Table 3. Project Trip Generation

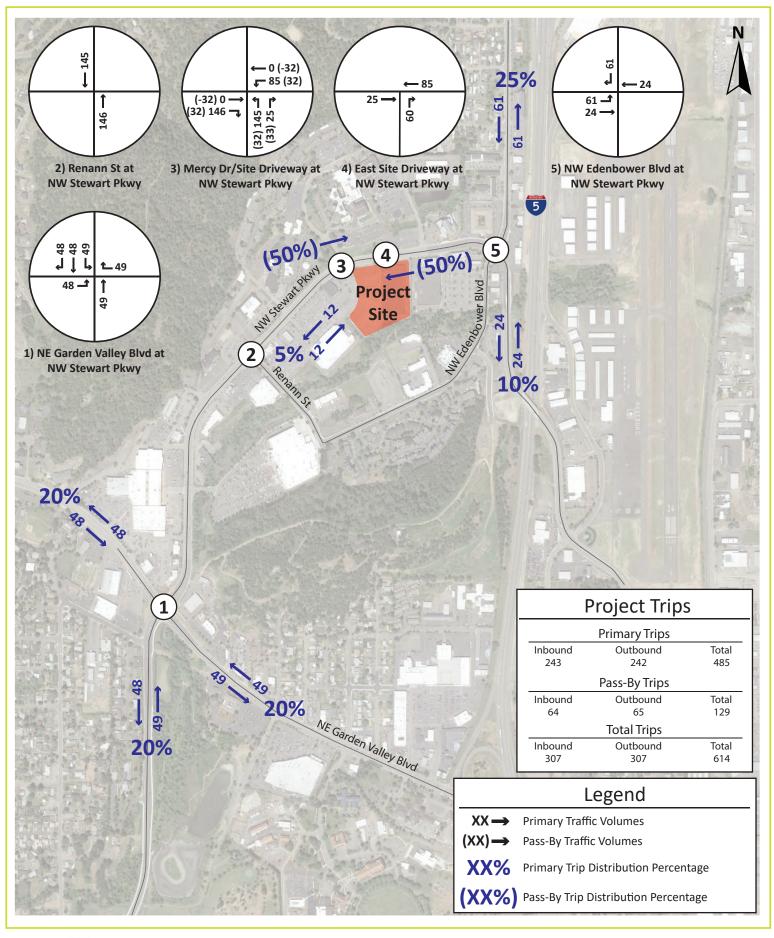
	Size	Total	Pass-By _	New-to-Network Trips			
Land Use Category	(1,000-sq ft)	Trips	Trips	Enter	Exit	Total	
Discount Supermarket	74.00	614	129	242	242	485	
Free-Standing Discount Store	85.00	411	70	171	170	341	
Net Increase Project Trips	-	203	59	72	72	144	

3.2 Site Traffic Distribution and Assignment

For this study, the regional distribution of traffic to and from the proposed project was estimated based on the patterns of traffic movement evidenced from PM peak hour traffic counts that were collected on October 28, 2020 at the following intersections:

- NW Stewart Parkway at NW Edenbower Boulevard
- NW Stewart Parkway at W Mercy Drive/Site Driveway
- NW Stewart Parkway at Renann Street
- NW Stewart Parkway at NW Garden Valley Boulevard

The regional traffic distribution percentages and site traffic assignment for the proposed development during the PM peak period are shown on **Figure 5**.





WinCo Foods Roseburg, Oregon Traffic Scoping Analysis

Figure 5
PM Peak Hour Site Generated
Traffic Volumes

4 FUTURE TRAFFIC CONDITIONS

4.1 Roadway Network Improvements

The City of Roseburg's Transportation System Plan (TSP) identifies several improvements within the study area that could be constructed in the next 20 years. Although none of these improvements are included in the financially-constrained portion of the TSP, funding may be identified in the future.

- NBP20a Garden Valley Boulevard Arterial Upgrade This concept proposed more detailed study
 of opportunities to improve traffic flow and provide multimodal accommodations.
- R2 Garden Valley Boulevard at Stewart Parkway Add eastbound and westbound dual left turn lanes from Garden Valley Boulevard to Stewart Parkway and dual southbound right-turn lanes from Stewart Parkway to Garden Valley Boulevard.
- R15 Edenbower Boulevard north of Stewart Parkway Extend northbound receiving lane to accommodate existing dual left turn lanes from Stewart Parkway to northbound Edenbower Boulevard.

Given that these improvements are not in the financially-constrained TSP, the operational analysis was performed without them. In the long term some of these improvements may improve traffic operations, particularly at the intersections of NW Stewart Parkway with Edenbower and Garden Valley Boulevards.

4.2 Future Traffic Volumes

Traffic volume forecasts were prepared for weekday PM peak hour conditions for the 2022 opening year and for subsequent years in five-year increments through 2042. The baseline (non-project-related) future traffic volume forecasts were developed using data provided by ODOT from the Roseburg regional travel demand model. Expected traffic volume growth between the model's 2010 base year and 2035 future planning horizon year was evaluated and annualized growth rates were developed and applied to each intersection for each time period. Estimated volumes for each year were smoothed between closely-spaced intersections. No pipeline projects were identified in the study area or included in the background volume forecasts.

Traffic volumes for the project were estimated as described in Chapter 3 and shown in Figure 5 and were added to the baseline traffic volumes for each future year scenario. The traffic volume calculations for the study area intersections with and without the project for each time period are included in **Appendix C**.

5 TRAFFIC OPERATIONS ANALYSIS

Traffic analyses were conducted to identify any deficiencies within the study area for the weekday PM peak hour in the 2021 base year and 2022 project opening year. Per City of Roseburg requirements, longer-term analysis of study area intersections was also conducted for the PM peak hour in 2027, 2032, 2037 and 2042. The traffic signal timing data for each of the study intersections currently operating under traffic signal control was provided by the Oregon Department of Transportation (ODOT), and included phasing, coordination, and timing data.

5.1 Traffic Analysis Methods and Assumptions

5.1.1 Methods and Assumptions

The acknowledged source for determining overall capacity for arterial segments and independent intersections is the current edition of the *Highway Capacity Manual* (HCM) published by the Transportation Research Board (TRB). Intersection analysis was performed using the Synchro software package. This software implements the methods of the 6th Edition HCM. For signalized operations, the LOS and individual V/C ratios are reported using the 6th Edition HCM and the overall V/C ratios are reported using the 2000 HCM. For unsignalized intersections all movements are reported. Per the requirements of ODOT's Analysis Procedures Manual, all saturation flow rates were set to 1,750 vehicles per hour per lane on all approaches. Existing count data was used to identify Peak Hour Factors (PHFs), as well as the heavy vehicle percentage, and bicycle and pedestrian activity.

5.1.2 Mobility Standards

The City of Roseburg uses two mobility standards to evaluate transportation system performance: a volume-to-capacity (v/c) ratio and levels of service (LOS).

The volume-to-capacity ratio is a traffic operational performance measure that evaluates the "degree of saturation" for traffic movements at an intersection. Many factors affect the volume of traffic an intersection can accommodate during a specific time interval. These factors include the number of lanes, lane widths, the type of signal phasing, the number of parking maneuvers on the adjacent street, etc. Based on these factors, the intersection (or individual lane group) is determined to have a total vehicle carrying capacity "c" for the analysis period. The analysis period volume "v" is compared to the calculated carrying capacity and presented as a ratio. If the v/c ratio is below 1.0, the demand volume is less than the maximum capacity. If the v/c ratio is over 1.0, the demand volume is exceeding the available carrying capacity.

Traffic operational performance can also be described in terms of Level of Service (LOS). LOS is a qualitative term describing operating conditions a driver will experience while traveling on a street or highway during a specific time interval. LOS ranges from A (very little delay) to F (long delays and congestion).

For signalized intersections, the overall LOS grade represents the weighted average vehicular delay (in seconds) of all movements at the intersection. For intersections under minor street stop-sign control, the LOS of the most difficult movement (typically the minor street left turn) is used to represent the intersection's performance. The LOS/delay criteria for stop sign-controlled intersections are different than for signalized intersections because driver expectation is that a signalized intersection is designed

to carry higher traffic volumes and experience greater delay. **Table 4** shows the Level of Service criteria for stop-controlled intersections and signalized intersections.

Table 4. Level of Service Criteria

Level of Service	Stop-Controlled Intersection Average Control Delay (seconds/vehicle)	Traffic Signal Intersection Average Control Delay (seconds/vehicle)
Α	≤ 10	≤ 10
В	> 10-15	> 10-20
С	> 15-25	> 20-35
D	> 25-35	> 35-55
E	> 35-50	> 55-80
F	> 50	> 80

For city streets, Roseburg uses the following traffic operational performance standards that have been adopted in the City's Municipal Code (Title 12, Section 06.020 C). Volume-to-capacity standards vary depending on the functional classification of the street(s) being analyzed, while LOS varies based on signalized or unsignalized traffic control.

• Volume to Capacity Ratio:

Arterial: 0.85 Collector: 0.90 Local: 0.95

• Level of Service Standard:

Signalized intersection: LOS D Non-signalized intersection: LOS E

5.2 Intersection Analysis

Intersection operations analysis was conducted for the following scenarios:

- Existing 2020 traffic volumes
- Projected background traffic volumes without the *WinCo Foods* project for five year increments between 2022 and 2042, inclusive
- Projected traffic volumes with the WinCo Foods project for five year increments between 2022 and 2042, inclusive

Detailed traffic operations analysis worksheets are included in **Appendix D** for conditions without the project and **Appendix E** for conditions with the project.

5.2.1 2020 Intersection Operations and Queuing Analysis

2020 Operations

Table 5 presents a summary of traffic operations analysis results for study intersections in the 2020 base year PM peak hour. These results are based on the traffic volumes collected in October 2020 and adjusted as previously described. As indicated in the table, all study area intersections are currently

operating withing their proscribed mobility standard, although the intersection of NW Stewart Parkway with Garden Valley Boulevard is approaching the V/C threshold of 0.85 for an arterial road.

Table 5. 2020 Existing and 2022 Project Opening PM Peak Hour Intersection Operations

					Projected 2022					
		Ва	se Year 20	020		No Build			Build	
Intersection/Movement	Mobility Standard	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS
NW Stewart Parkway at NW Edenbower Boulevard ¹	V/C=0.85 LOS D	0.69	53.9	D	0.71	56.1	E	0.74	58.8	E
NW Stewart Parkway at E Site Driveway ² Northbound Right	V/C=0.85 LOS E							0.18	14.2	В
NW Stewart Parkway at W Mercy Drive/Site Driveway ¹	V/C=0.85 LOS D	0.45	15.2	В	0.48	15.4	В	0.71	22.4	С
NW Stewart Parkway at Renann Street ¹	V/C=0.85 LOS D	0.49	17.4	В	0.50	17.4	В	0.56	17.4	В
NW Stewart Parkway at NW Garden Valley Boulevard ¹	V/C=0.85 LOS D	0.78	36.8	D	0.79	38.7	D	0.84	46.8	D

^{1.} Traffic Signal Control

2020 Queuing

A vehicle queue represents the number of stopped vehicles waiting to travel through an intersection. In this analysis, the 95th percentile maximum back of queue for the PM peak hour has been estimated at the study locations. A 95th percentile queue reflects the length (in feet) that has only a 5-percent probability of being exceeded during the peak hour. The 95th percentile queue is a useful parameter for determining the appropriate length of turn pockets or to determine if queuing could occasionally exceed available storage or interfere with traffic operations at upstream driveways or other intersections. However, is not typical of what the average driver would experience. The queue analysis was performed with the SimTraffic software package using the average of five simulations.

Table 6 presents the results of 95th percentile queuing analysis for 2020 traffic conditions at study area intersections. The table includes specific information for all movements along NW Stewart Parkway and for movements to and from the project area at the intersections of NW Stewart Parkway with both Edenbower Boulevard and Garden Valley Boulevard. As indicated in the table, several intersections are currently experiencing traffic queues that exceed available vehicle storage. Of particular relevance to the project is the eastbound left turn from NW Stewart Parkway to northbound Edenbower Boulevard where the 95th percentile 2020 PM peak hour queue is estimated fill the available storage spilling back to approximately the intersection with E Mercy Drive. At all other locations where queues are expected to exceed storage, the impact is expected to be of limited length and duration.

^{2.} Two-way Stop Control

Table 6. 2020 Existing and 2022 Project Opening PM Peak Hour Queuing

		95 th Perc	entile Queues	Queues (Feet)		
	Available	2020	2022			
Intersection/Movement	Storage (ft)	No Build	No Build	Build		
NW Stewart Parkway at NW Edenbower Boulevard						
Eastbound Left	565 ft	581 ft	448 ft	636 ft		
Eastbound Through	1,315 ft	109 ft	121 ft	194 ft		
Eastbound Through/Right	1,315 ft	130 ft	141 ft	169 ft		
Westbound Left	200 ft	171 ft	172 ft	181 ft		
Westbound Through	1,500+ ft	163 ft	172 ft	190 ft		
Northbound Left	125 ft	129 ft	119 ft	127 ft		
Southbound Right	125 ft	139 ft	149 ft	157 ft		
NW Stewart Parkway at E Site Driveway						
Northbound Right	120 ft			87 ft		
NW Stewart Parkway at W Mercy Drive/Site Driveway						
Eastbound Left	200 ft	83 ft	104 ft	139 ft		
Eastbound Through	1,040 ft	248 ft	278 ft	370 ft		
Westbound Left	200 ft	64 ft	63 ft	149 ft		
Westbound Through/Right	1,315 ft	165 ft	180 ft	205 ft		
Northbound Left	120 ft	59 ft	55 ft	112 ft		
Northbound Through/Right	400 ft	67 ft	60 ft	285 ft		
Southbound Left	100 ft	135 ft	137 ft	128 ft		
Southbound Through/Right	230 ft	139 ft	147 ft	128 ft		
NW Stewart Parkway at Renann Street						
Eastbound Left	50 ft	47 ft	52 ft	52 ft		
Eastbound Through/Right	150 ft	53 ft	53 ft	50 ft		
Westbound Left	125 ft	168 ft	171 ft	168 ft		
Westbound Through/Right	910 ft	255 ft	283 ft	247 ft		
Northbound Left	150 ft	46 ft	44 ft	62 ft		
Northbound Through	1,300 ft	179 ft	203 ft	215 ft		
Northbound Right	, 75 ft	79 ft	95 ft	99 ft		
Southbound Left	150 ft	74 ft	79 ft	105 ft		
Southbound Through	1,040 ft	215 ft	204 ft	228 ft		
Southbound Right	, 75 ft	44 ft	38 ft	41 ft		
NW Stewart Parkway at NW Garden Valley Boulevard						
Eastbound Left	225 ft	273 ft	280 ft	284 ft		
Westbound Right	140 ft	164 ft	162 ft	159 ft		
Northbound Through	1,500+ ft	167 ft	181 ft	207 ft		
Southbound Left	465 ft	310 ft	333 ft	405 ft		
Southbound Through	975 ft	308 ft	322 ft	332 ft		
Southbound Right	100 ft	148 ft	147 ft	146 ft		

5.2.2 2022 Intersection Operations and Queuing Analysis

2022 Operations

Table 5 also presents a summary of traffic operations analysis results for 2022 PM peak hour traffic conditions with and without the proposed *WinCo Foods* project. As indicated in the table, all intersections are expected to operate within their applicable V/C standard. However, the intersection of NW Stewart Parkway at NW Edenbower Boulevard is expected to exceed the applicable level of service threshold during the PM peak hour with and without the project. Traffic is expected to operate at LOS E with between 55 and 60 seconds of average vehicular delay, depending on the analysis scenario. A slight modification to the existing signal cycle length was tested to determine if operational delay could be

reduced. It was determined the LOS could be improved from LOS E to LOS D for conditions with and without the project if the cycle length were increased from 110 to 130 seconds. However, this change may have adverse impacts on intersection queuing and/or operations at other intersections in the vicinity and would need to be more fully studied before implementation.

2022 Intersection Queuing

Table 6 also presents the results of 95th percentile queuing analysis for 2022 PM peak hour traffic conditions at study area intersections with or without the *WinCo Food* project. As indicated in the table, several intersections are expected to experience traffic queues that exceed available vehicle storage. Of particular relevance to the project is the eastbound left turn from NW Stewart Parkway to northbound Edenbower Boulevard where the projected queue with the project is expected to be over 600 feet in length, extending into the intersection with E Mercy Drive. At all other locations where queues are expected to exceed storage in the 2022 PM peak hour, the impact is expected to be of limited length and duration and in most instances would occur with or without the project.

5.2.3 2027 Intersection Operations and Queuing Analysis

2027 Operations

Table 7 presents a summary of traffic operations analysis results for 2027 PM peak hour traffic conditions with and without the proposed *WinCo Foods* project. As indicated in the table, the intersection of NW Stewart Parkway with Edenbower Boulevard is expected to exceed the mobility standard of LOS D for the 2027 PM peak hour condition with or without the project. The V/C standard at this location would still be met. Additionally, the V/C standard at the intersection of NW Stewart Parkway with Garden Valley Boulevard is expected to be met during the 2027 PM peak hour without the project and exceeded with the project. The LOS standard at this location would be met under both scenarios. All other intersections are expected to operate within their applicable V/C and LOS standard.

Table 7, 2027 Project Opening PM Peak Hour Intersection	ction	our Intersection (perations
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		Projected 2027						
			No Build			Build		
Intersection/Movement	Mobility Standard	V/C	Delay	LOS	V/C	Delay	LOS	
NW Stewart Parkway at NW Edenbower Boulevard ¹	V/C=0.85 LOS D	0.76	59.6	E	0.82	68.4	E	
NW Stewart Parkway at E Site Driveway ² Northbound Right	V/C=0.85 LOS E				0.20	15.2	С	
NW Stewart Parkway at W Mercy Drive/Site Driveway ¹	V/C=0.85 LOS D	0.53	15.8	В	0.78	23.8	С	
NW Stewart Parkway at Renann Street ¹	V/C=0.85 LOS D	0.52	17.3	В	0.58	17.4	В	
NW Stewart Parkway at NW Garden Valley Boulevard ¹	V/C=0.85 LOS D	0.85	45.0	D	0.93	53.7	D	

- 1. Traffic Signal Control
- 2. Two-way Stop Control

2027 Queuing

Table 8 presents the results of 95th percentile queuing analysis for 2027 PM peak hour traffic conditions at study area intersections with or without the *WinCo Food* project. As indicated in the table, eastbound left turns from NW Stewart Parkway to northbound Edenbower Boulevard are expected to exceed available storage by about 100 feet without the project. Project-related impacts are expected to be less due to a redistribution of intersection green time to accommodate the increased traffic volumes with the project. Other project-related queuing impacts are either similar to the without project condition or would not substantively increase expected queuing impacts. One exception is the southbound left turn at the intersection of NW Stewart Parkway with Garden Valley Boulevard which is expected to fill the available storage without the project and exceed available storage with the project.

Table 8. 2027 Project Opening PM Peak Hour Queuing

	Available	95 th Percentile Queues (Feet)			
Intersection/Movement	Storage (ft)	No Build	Build		
NW Stewart Parkway at NW Edenbower Boulevard					
Eastbound Left	565 ft	612 ft	718 ft		
Eastbound Through	1,315 ft	157 ft	187 ft		
Eastbound Through/Right	1,315 ft	177 ft	206 ft		
Westbound Left	200 ft	222 ft	231 ft		
Westbound Through	1,500+ ft	277 ft	263 ft		
Northbound Left	125 ft	143 ft	132 ft		
Southbound Right	125 ft	162 ft	173 ft		
NW Stewart Parkway at E Site Driveway					
Northbound Right	120 ft		82 ft		
NW Stewart Parkway at W Mercy Drive/Site Driveway					
Eastbound Left	200 ft	119 ft	183 ft		
Eastbound Through	1,040 ft	311 ft	422 ft		
Westbound Left	200 ft	84 ft	191 ft		
Westbound Through/Right	1,315 ft	216 ft	235 ft		
Northbound Left	120 ft	56 ft	115 ft		
Northbound Through/Right	400 ft	69 ft	284 ft		
Southbound Left	100 ft	128 ft	135 ft		
Southbound Through/Right	230 ft	136 ft	165 ft		
NW Stewart Parkway at Renann Street					
Eastbound Left	50 ft	54 ft	51 ft		
Eastbound Through/Right	150 ft	62 ft	52 ft		
Westbound Left	125 ft	168 ft	169 ft		
Westbound Through/Right	910 ft	291 ft	249 ft		
Northbound Left	150 ft	66 ft	66 ft		
Northbound Through	1,300 ft	203 ft	229 ft		
Northbound Right	75 ft	98 ft	104 ft		
Southbound Left	150 ft	89 ft	84 ft		
Southbound Through	1,040 ft	236 ft	238 ft		
Southbound Right	75 ft	47 ft	31 ft		
NW Stewart Parkway at NW Garden Valley Boulevard					
Eastbound Left	225 ft	284 ft	266 ft		
Westbound Right	140 ft	162 ft	152 ft		
Northbound Through	1,500+ ft	195 ft	212 ft		
Southbound Left	465 ft	554 ft	665 ft		
Southbound Through	975 ft	348 ft	417 ft		
Southbound Right	100 ft	143 ft	144 f t		

5.2.4 2032 Intersection Operations and Queuing Analysis

2032 Operations

Table 9 presents a summary of traffic operations analysis results for 2032 PM peak hour traffic conditions with and without the proposed *WinCo Foods* project. As indicated in the table, the intersection of NW Stewart Parkway with Edenbower Boulevard is expected to exceed the mobility standard of LOS D for the 2032 PM peak hour condition with or without the project. The V/C standard at this location would also be exceeded. Additionally, the V/C standard at the intersection of NW Stewart Parkway with Garden Valley Boulevard is expected to be exceeded during the 2032 PM peak hour with or without the project. The LOS standard at this location would be exceeded with the project. All other intersections are expected to operate at or within their applicable V/C or LOS standard.

Projected 2032 No Build Build Mobility V/C LOS V/C Intersection/Movement Standard Delay Delay LOS NW Stewart Parkway at NW Edenbower V/C=0.85 E 0.91 F 0.87 79.3 82.9 Boulevard1 LOS D NW Stewart Parkway at E Site Driveway² V/C=0.85 0.22 16.3 С Northbound Right LOS E NW Stewart Parkway at W Mercy Drive/Site V/C=0.85 0.59 16.7 В 0.85 26.3 С Driveway¹ LOS D V/C=0.85 NW Stewart Parkway at Renann Street¹ 0.54 17.4 В 0.60 17.6 В LOS D NW Stewart Parkway at NW Garden Valley V/C=0.85 0.93 51.5 D 1.01 62.9 Ē Boulevard¹ LOS D

Table 9. 2032 Project Opening PM Peak Hour Intersection Operations

2032 Queuing

Table 10 presents the results of 95th percentile queuing analysis for 2032 PM peak hour traffic conditions at study area intersections with or without the *WinCo Food* project. As indicated in the table, eastbound left turns from NW Stewart Parkway to northbound Edenbower Boulevard would exceed available storage by several hundred feet with or without the project. The southbound right turns are also expected to exceed available storage for conditions with or without the project.

Other project-related queuing impacts are either similar to the without project condition or would not substantively increase expected queuing impacts with one exception. The southbound left turn from NW Stewart Parkway to eastbound Garden Valley Boulevard is expected to exceed available storage with or without the project.

^{1.} Traffic Signal Control

^{2.} Two-way Stop Control

Table 10. 2032 Project Opening PM Peak Hour Queuing

	Available	95 th Percentile Queues (Feet)			
Intersection/Movement	Storage (ft)	No Build	Build		
NW Stewart Parkway at NW Edenbower Boulevard					
Eastbound Left	565 ft	1,081 feet	930 feet		
Eastbound Through	1,315 ft	189 feet	221 feet		
Eastbound Through/Right	1,315 ft	213 feet	235 feet		
Westbound Left	200 ft	245 feet	259 feet		
Westbound Through	1,500+ ft	377 feet	510 feet		
Northbound Left	125 ft	165 feet	177 feet		
Southbound Right	125 ft	173 feet	177 feet		
NW Stewart Parkway at E Site Driveway					
Northbound Right	120 ft		120 feet		
NW Stewart Parkway at W Mercy Drive/Site Driveway					
Eastbound Left	200 ft	84 feet	183 feet		
Eastbound Through	1,040 ft	310 feet	438 feet		
Westbound Left	200 ft	94 feet	208 feet		
Westbound Through/Right	1,315 ft	220 feet	257 feet		
Northbound Left	120 ft	55 feet	114 feet		
Northbound Through/Right	400 ft	69 feet	284 feet		
Southbound Left	100 ft	137 feet	137 feet		
Southbound Through/Right	230 ft	180 feet	150 feet		
NW Stewart Parkway at Renann Street					
Eastbound Left	50 ft	53 feet	50 feet		
Eastbound Through/Right	150 ft	61 feet	46 feet		
Westbound Left	125 ft	172 feet	170 feet		
Westbound Through/Right	910 ft	249 feet	239 feet		
Northbound Left	150 ft	68 feet	60 feet		
Northbound Through	1,300 ft	207 feet	208 feet		
Northbound Right	75 ft	99 feet	108 feet		
Southbound Left	150 ft	99 feet	107 feet		
Southbound Through	1,040 ft	229 feet	255 feet		
Southbound Right	75 ft	53 feet	42 feet		
NW Stewart Parkway at NW Garden Valley Boulevard					
Eastbound Left	225 ft	271 feet	251 feet		
Westbound Right	140 ft	167 feet	153 feet		
Northbound Through	1,500+ ft	195 feet	237 feet		
Southbound Left	465 ft	585 feet	573 feet		
Southbound Through	975 ft	370 feet	392 feet		
Southbound Right	100 ft	147 feet	146 feet		

5.2.5 2037 Intersection Operations and Queuing Analysis

2037 Operations

Table 11 presents a summary of traffic operations analysis results for 2037 PM peak hour traffic conditions with and without the proposed *WinCo Foods* project. As indicated in the table, the intersections of NW Stewart Parkway with Edenbower Boulevard and with Garden Valley Boulevard are both expected to exceed the mobility standard of LOS D for the 2037 PM peak hour condition with or without the project. The V/C standard at these locations would also be exceeded for the same scenarios. The intersection of NW Stewart Parkway with E Mercy Drive is expected to exceed its V/C threshold during the 2037 PM peak hour with the project but to meet the applicable LOS standard. All other intersections are expected to operate within their applicable V/C or LOS standard.

Table 11. 2037 Project Opening PM Peak Hour Intersection Operations

		Projected 2037						
			No Build			Build		
Intersection/Movement	Mobility Standard	V/C	Delay	LOS	V/C	Delay	LOS	
NW Stewart Parkway at NW Edenbower Boulevard ¹	V/C=0.85 LOS D	0.95	92.3	F	0.98	96.1	F	
NW Stewart Parkway at E Site Driveway ² Northbound Right	V/C=0.85 LOS E				0.23	17.6	С	
NW Stewart Parkway at W Mercy Drive/Site Driveway ¹	V/C=0.85 LOS D	0.65	17.9	В	0.91	29.8	С	
NW Stewart Parkway at Renann Street ¹	V/C=0.85 LOS D	0.57	17.4	В	0.62	17.7	В	
NW Stewart Parkway at NW Garden Valley Boulevard ¹	V/C=0.85 LOS D	1.02	64.8	E	1.10	85.3	F	

- 1. Traffic Signal Control
- 2. Two-way Stop Control

2037 Queuing

Table 12 presents the results of queuing analysis for 2037 PM peak hour traffic at study area intersections. As indicated in the table, eastbound left turns at the NW Stewart Parkway/Edenbower Boulevard intersection are expected to substantially exceed available storage with or without the project, as would the southbound left turn from NW Stewart Parkway to eastbound Garden Valley Boulevard. Additionally, the northbound right turn queue at the new site access driveway will likely backup into the parking area. This would not affect the public street. Other project-related queuing impacts are either similar to the without project condition or would not substantively alter expected queuing impacts from the no-project condition.

Table 12. 2037 Project Opening PM Peak Hour Queuing

	Available	95 th Percentile	e Queues (Feet)		
Intersection/Movement	Storage (ft)	No Build	Build		
NW Stewart Parkway at NW Edenbower Boulevard					
Eastbound Left	565 ft	1,156 ft	1,312 ft		
Eastbound Through	1,315 ft	206 ft	258 ft		
Eastbound Through/Right	1,315 ft	231 ft	263 ft		
Westbound Left	200 ft	240 ft	233 ft		
Westbound Through	1,500+ ft	625+ ft	625+ ft		
Northbound Left	125 ft	163 ft	158 ft		
Southbound Right	125 ft	179 ft	182 ft		
NW Stewart Parkway at E Site Driveway					
Northbound Right	120 ft		254 ft		
NW Stewart Parkway at W Mercy Drive/Site Driveway					
Eastbound Left	200 ft	137 ft	201 ft		
Eastbound Through	1,040 ft	369 ft	459 ft		
Westbound Left	200 ft	112 ft	199 ft		
Westbound Through/Right	1,315 ft	262 ft	242 ft		
Northbound Left	120 ft	57 ft	114 ft		
Northbound Through/Right	400 ft	78 ft	272 ft		
Southbound Left	100 ft	136 ft	137 ft		
Southbound Through/Right	230 ft	175 ft	178 ft		

Table 12 Continued. 2037 Project Opening PM Peak Hour Queuing

	Available	95 th Percentile Queues (Feet)			
Intersection/Movement	Storage (ft)	No Build	Build		
NW Stewart Parkway at Renann Street					
Eastbound Left	50 ft	54 ft	47 ft		
Eastbound Through/Right	150 ft	58 ft	51 ft		
Westbound Left	125 ft	169 ft	171 ft		
Westbound Through/Right	910 ft	251 ft	272 ft		
Northbound Left	150 ft	57 ft	58 ft		
Northbound Through	1,300 ft	179 ft	225 ft		
Northbound Right	75 ft	107 ft	105 ft		
Southbound Left	150 ft	113 ft	76 ft		
Southbound Through	1,040 ft	243 ft	230 ft		
Southbound Right	75 ft	42 ft	38 ft		
NW Stewart Parkway at NW Garden Valley Boulevard					
Eastbound Left	225 ft	263 ft	250 ft		
Westbound Right	140 ft	162 ft	165 ft		
Northbound Through	1,500+ ft	242 ft	222 ft		
Southbound Left	465 ft	719 ft	765 ft		
Southbound Through	975 ft	401 ft	395 ft		
Southbound Right	100 ft	148 ft	145 ft		

5.2.6 2042 Intersection Operations and Queuing Analysis

2042 Operations

Table 13 presents a summary of traffic operations analysis results for 2042 PM peak hour traffic conditions with and without the proposed *WinCo Foods* project. As indicated in the table, the intersections of NW Stewart Parkway with Edenbower Boulevard and with Garden Valley Boulevard are both expected to exceed the mobility standard of LOS D for the 2042 PM peak hour condition with or without the project. The V/C standard at these locations would also be exceeded for the same scenarios. The intersection of NW Stewart Parkway with E Mercy Drive is expected to exceed its V/C threshold during the 2042 PM peak hour with the project but to meet the applicable LOS standard. All other intersections are expected to operate within their applicable V/C or LOS standard.

Table 13. 2042 Project Opening PM Peak Hour Intersection Operations

			Projected 2042				
			No Build			Build	
Intersection/Movement	Mobility Standard	V/C	Delay	LOS	V/C	Delay	LOS
NW Stewart Parkway at NW Edenbower Boulevard ¹	V/C=0.85 LOS D	1.04	111.3	F	1.07	115.3	F
NW Stewart Parkway at E Site Driveway ² Northbound Right	V/C=0.85 LOS E	-	-	-	0.25	19.1	С
NW Stewart Parkway at W Mercy Drive/Site Driveway ¹	V/C=0.85 LOS D	0.72	19.6	В	1.00	35.9	D
NW Stewart Parkway at Renann Street ¹	V/C=0.85 LOS D	0.59	17.4	В	0.65	17.8	В
NW Stewart Parkway at NW Garden Valley Boulevard ¹	V/C=0.85 LOS D	1.11	85.1	F	1.19	97.8	F

- 1. Traffic Signal Control
- 2. Two-way Stop Control

2042 Queuing

Table 14 presents the results of 95th percentile queuing analysis for 2042 PM peak hour traffic conditions at study area intersections with or without the *WinCo Food* project. As indicated in the table, eastbound left turns from NW Stewart Parkway to northbound Edenbower Boulevard are expected to substantially exceed available storage with or without the project. Available storage for southbound right turns would also be exceeded with or without the project, as would north and westbound lefts. Additionally, the northbound right turn queue at the new site access driveway will likely backup into the parking area. This would not affect the public street. Other project-related queuing impacts are either similar to the without project condition or would not substantively increase expected queuing impacts.

Table 14. 2042 Project Opening PM Peak Hour Queuing

	Available	95 th Percentile Queues (Feet)			
Intersection/Movement	Storage (ft)	No Build	Build		
NW Stewart Parkway at NW Edenbower Boulevard					
Eastbound Left	565 ft	1,232 ft	1,355 ft		
Eastbound Through	1,315 ft	239 ft	307 ft		
Eastbound Through/Right	1,315 ft	267 ft	306 ft		
Westbound Left	200 ft	229 ft	229 ft		
Westbound Through	1,500+ ft	625+ ft	625+ ft		
Northbound Left	125 ft	162 ft	172 ft		
Southbound Right	125 ft	177 ft	167 ft		
NW Stewart Parkway at E Site Driveway					
Northbound Right	120 ft	-	236 ft		
NW Stewart Parkway at W Mercy Drive/Site Driveway					
Eastbound Left	200 ft	158 ft	167 ft		
Eastbound Through	1,040 ft	454 ft	512 ft		
Westbound Left	200 ft	141 ft	227 ft		
Westbound Through/Right	1,315 ft	266 ft	276 ft		
Northbound Left	120 ft	58 ft	114 ft		
Northbound Through/Right	400 ft	81 ft	279 ft		
Southbound Left	100 ft	140 ft	144 ft		
Southbound Through/Right	230 ft	196 ft	229 ft		

Table 14 Continued. 2042 Project Opening PM Peak Hour Queuing

	Available	95 th Percentile Queues (Feet)	
Intersection/Movement	Storage (ft)	No Build	Build
NW Stewart Parkway at Renann Street			
Eastbound Left	50 ft	49 ft	45 ft
Eastbound Through/Right	150 ft	50 ft	55 ft
Westbound Left	125 ft	170 ft	170 ft
Westbound Through/Right	910 ft	259 ft	290 ft
Northbound Left	150 ft	57 ft	91 ft
Northbound Through	1,300 ft	187 ft	237 ft
Northbound Right	75 ft	108 ft	108 ft
Southbound Left	150 ft	95 ft	98 ft
Southbound Through	1,040 ft	239 ft	231 ft
Southbound Right	75 ft	52 ft	41 ft
NW Stewart Parkway at NW Garden Valley Boulevard			
Eastbound Left	225 ft	255 ft	249 ft
Westbound Right	140 ft	168 ft	161 ft
Northbound Through	1,500+ ft	231 ft	276 ft
Southbound Left	465 ft	477 ft	501 ft
Southbound Through	975 ft	556 ft	561 ft
Southbound Right	100 ft	128 ft	126 ft

6 SUMMARY AND CONCLUSIONS

6.1 Project Summary

WinCo Foods is proposing to rebuild an existing apppromximate 85,000-square foot vacant commercial building into a WinCo Foods store in Roseburg, Oregon. The proposed WinCo Foods store will be approximately 74,000-square feet in size and will be located on NW Stewart Parkway to the west of the intersection with Edenbower Boulevard and opposite the Mercy Hospital and Medical Center.

As part of the renovation a portion of the existing structure nearest to NW Stewart Parkway will be removed to provide additional parking north of the building. Access to the project will be provided by two existing driveways along NW Stewart Parkway. One of these driveways connects to the existing traffic signal-controlled intersection with W Mercy Drive and will provide for all turning movements. The other will be relocated to the east edge of the proposed WinCo Foods and will be limited to right in/right out movements. The project is anticipated to open in 2022.

6.2 Operational Analysis Summary

Operational analysis was conducted for the PM peak hour for the existing 2020, projected 2022 project opening year with and without project completion, and projected traffic volumes with the *WinCo Foods* project for five year increments between 2022 and 2042, inclusive.

- All study intersections in 2022 are projected to operate at the established V/C ratio and LOS standards except the LOS at NW Stewart Parkway/NW Edenbower Boulevard. With a lengthened cycle length this intersection is expected to improve to LOS D.
- In the 2042 horizon the intersections of NW Stewart Parkway with both NW Edenbower Boulevard and NW Garden Valley Boulevard are projected to operate below the established V/C and LOS operational standards. The intersection of NW Stewart Parkway/W Mercy Drive is expected to exceed the V/C standard but meet the LOS standard.
- The City's TSP has identified improvements at the NW Stewart Parkway/NW Edenbower Boulevard and NW Stewart Parkway/NW Garden Valley Boulevard intersections as part of its Tier 2 unconstrained improvement program. For the NW Stewart Parkway/W Mercy Drive intersection the increase in traffic associated with the project is such that the traffic signal timings should be reassessed. With optimization the LOS and V/C ratio would both be expected to improve, although the V/C ratio would remain above 0.85.
- The eastbound left-turn queue at NW Stewart Parkway/NW Edenbower Boulevard is currently
 extending nearly to the E Marcy Drive intersection and is expected to grow through the 20-year
 horizon. This queue is not projected to extend to the NW Stewart Parkway/W Mercy Drive
 intersection.

6.3 Conclusions and Recommendations

The analysis of PM peak hour traffic operations for the 2022 project opening year and 2042 long term horizon year indicate that growing congestion will be experienced, and extensive queuing will occur. Operational failure during the PM peak hour at the intersection of NW Stewart Parkway with Edenbower Boulevard is expected to occur by the 2022 opening year horizon with or without the

addition of project-related traffic. Some adjustments to the traffic signal cycle length and phasing could be considered to provide a few more years of service at this intersection but system improvements will be required over the longer-term. This is not a project-specific impact and no project-related mitigation is suggested.

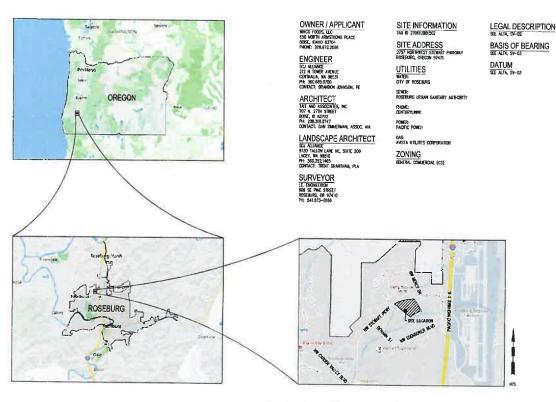
Due to existing and expected traffic queuing along NW Stewart Parkway, it is suggested that the existing unsignalized driveway accessing the project to the east of the NW Stewart Parkway/W Mercy Drive intersection be relocated further east to access new on-site parking and be constructed to provide for right-in and right-out movements only.

Inbound and outbound left-turning traffic at the existing stop-controlled driveway will be diverted to the signalized intersection of NW Stewart Parkway with W Mercy Drive. This diversion can be accommodated without adversely impacting traffic operations at this location. Concern has been expressed by the City regarding the impaired sight distance for northbound right turns at this intersection due to the presence of another commercial building. Traffic operations analysis at this location was prepared assuming that no right turns on red would occur. Operations analysis results indicate that if the city were to install a right turn on red prohibition for this movement, it would not adversely affect traffic performance and could improve safety. In addition, the new site access driveway will also accommodate northbound right turns and has adequate sight distance.

COMMERCIAL DEVELOPMENT FOR



ON-SITE CIVIL SITE PLAN REVIEW (SPR) ROSEBURG, OR



A PORTION OF NE QUARTER OF SEC. 11 AND THE NW QUARTER OF SEC 12, T27S., R6W., W.M.		
ROSEBURG, OR		

	SHEET INDEX
SHEET	SHEET TITLE
CIVIL	
CV-CI	COVER SHEET
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SV-02	ALTA SURVEY
SV-03	ALTA SURVEY
EC-01	EROSION CONTROL PLAN
DM-01	DEMOLITION PLAN
59-01	SITE PLAN
PV-01	PAWING PLAN
P¥-02	PAMNG DETAILS
CG-81	OVERALL GRADING PLAN
DG-02	DETAILED GRADING PLAN
MU-01	MASTER UTITUTY PLAN
SD-01	STORWWATER PLAN
NT-01	WATER PLAN
WT-02	WATER PROFILES

SCHER PROFILES

NORTHINEST LANDSCAPE PLAN
NORTHEAST LANDSCAPE PLAN
SOUTHEAST LANDSCAPE PLAN
SOUTHEAST LANDSCAPE PLAN
DETAL SHEET



COVER SHEET

CV-01

28, 2021. ILE-BOTROM — User brandom jahnesa. 24-FSQ-SRA-KO-SCLUOV/PROJECTSVJSTO TAIT AND ASSOCIATES, INC.VSD::35 WARD FGGGS RIGES

CALL BEFORE YOU DIG

THE CONTRACTOR SAVIL BE HALLY RESPONSIBLE FOR THE LOCATION
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SCJ ALLIANCE - GENERAL CONSTRUCTION NOTES:

- 1. ALL WORK, WORKMANSHIP AND MATERIALS FOR THIS PROJECT SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE FOLLOWING MANUAL(S) AND DOCUMENT(S):
- OREGON STATE DEPARTMENT OF TRANSPORTATION (ODOT) STANDARD SPECIFICATIONS FOR CONSTRUCTION (2018) (HTTPS: //WWW.OREGON.GOV/ODOT/BUSINESS/PAGES/STANDARD_SPECIFICATIONS.ASPX)
- ROSEBURG URBAN SANITARY AUTHORITY CODES (HTTPS://WWW.RUSA-OR.ORG/RUSA-CODE)
- ROSEBURG MUNICIPAL CODE (HTTPS://LIBRARY.MUNICODE.COM/OR/ROSEBURG/)
- 2. ALL GOVERNMENTAL SAFETY REGULATIONS SHALL BE STRICTLY ADHERED TO INCLUDING OSHA.
- 3. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DULY NOTIFY THE CITY OF ROSEBURG IN ADVANCE OF THE COMMENCEMENT OF ANY AUTHORIZED WORK AND TO SCHEDULE REQUIRED INSPECTIONS. ANY REQUIRED INSPECTION TEST WILL BE PERFORMED AT THE CONTRACTOR'S EXPENSE.
- 4. THE APPROVAL OF THESE PLANS BY THE CITY OF ROSEBURG DOES NOT RELIEVE THE CONTRACTOR OR DEVELOPER OF THE RESPONSIBILITY TO COMPLY WITH THE REQUIREMENTS OF OTHER GOVERNING AGENCIES.

<u>CAUTION - NOTICE TO CONTRACTOR</u>

- 5. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON THE PROJECT SURVEY AND OTHER RECORDS OF UTILITIES. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL CALL 811 48 HOURS PRIOR TO PLANNED EXCAVATIONS.
- 6. THE DESIGN SHOWN IS BASED UPON THE ENGINEER'S UNDERSTANDING OF THE EXISTING CONDITIONS. THE EXISTING CONDITIONS SHOWN IN THIS PLAN SET ARE BASED UPON COMPILED SURVEY DATA. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING FIELD CONDITIONS PRIOR TO BIDDING THE PROPOSED WORK IMPROVEMENTS. IF CONFLICTS ARE DISCOVERED, THE CONTRACTOR SHALL NOTIFY THE OWNER OR OWNER'S REPRESENTATIVE.
- 7. EXISTING UTILITIES ARE SHOWN FOR REFERENCE ONLY. THE CONTRACTOR SHALL VERIFY EXACT LOCATION, DIAMETER, LENGTH, CONDITION, PIPE TYPE, SLOPE AND VERTICAL AND HORIZONTAL ALIGNMENT OF THE PROPOSED POINTS OF CONNECTION PRIOR TO CONNECTION AND REPORT ANY DISCREPANCIES TO ENGINEER PRIOR TO CONSTRUCTION.
- 8. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL OBTAIN ALL NECESSARY LOCAL, STATE, AND FEDERAL APPROVALS AND PERMITS.
- 9. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE A COPY OF THE APPROVED PLANS, SPECIFICATIONS, AND CONTRACT DOCUMENTS AT THE CONSTRUCTION SITE AT ALL TIMES.
- 10. CONSTRUCTION SIGNING AND TRAFFIC CONTROL SHALL BE PER THE CURRENT COPY OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- 11. ALL VEHICLES AND EQUIPMENT SHALL BE KEPT WITHIN THE WORK AREAS ESTABLISHED FOR THAT WORK SHIFT UNLESS TRAVELING TO OR FROM THE SITE. UNDER NO CIRCUMSTANCES SHALL VEHICLES BE PARKED OR EQUIPMENT BE STORED OUTSIDE OF THESE AREAS.
- 12. OTHER CONSTRUCTION PROJECTS MAY OCCUR NEAR THE PROJECT SITE AND MAY BE IN PROGRESS CONCURRENTLY WITH THE PROJECT. THE CONTRACTOR SHALL COOPERATE AS NECESSARY AND NOT INTERFERE OR HINDER THE PROGRESS OR COMPLETION OF WORK BEING PERFORMED BY OTHER CONTRACTORS.
- 13. THE CONTRACTOR IS RESPONSIBLE FOR FURNISHING AND INSTALLING ALL MATERIALS, LABOR, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK SHOWN ON THESE DRAWINGS AND TO OBTAIN ACCEPTANCE BY THE CITY OF ROSEBURG AND THE PROJECT OWNER.
- 14. ALL AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO THEIR ORIGINAL "PRE CONSTRUCTION" STATE OR BETTER.
- 15. DRIVEWAY ACCESS AND UTILITY SERVICE TO EXISTING HOMES AND BUSINESSES SHALL BE MAINTAINED AT ALL TIMES.
- 16. CONTRACTOR SHALL HIRE AN ARBORIST TO INSPECT ALL EXISTING TREES AND PROVIDE A REPORT ON RECOMMENDATIONS FOR PRUNING OR REMOVAL. CONTRACTOR SHALL PRUNE OR REMOVE TREES PER ARBORIST'S REPORT. SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION.
- 17. GROUNDWATER WAS NOTED AT A DEPT OF 8 TO 12 FEET BELOW THE CURRENT GROUND SURFACE. SEE TERRACON GEOTECHNICAL REPORT DATED 12-31-2020.

TYPICAL	ABBREVIATIONS				
&	AND	FDC	FIRE DEPARTMENT CONNECTION	S	SOUTH OR SLOPE
Đ	ANGLE	FDN	FOUNDATION	SCHED	SCHEDULE
±	APPROXIMATELY	FF	FINISH FLOOR	SD, SDMH	STORM DRAIN, STORM DRAIN MANHOLE
	AT	FG	FINISHED GRADE	SE	SOUTHEAST
<u>@</u> မို	CENTERLINE	FH	FIRE HYDRANT	SECT	SECTION(S)
•	DEGREE	FIN	FINISH(ED)	SHT	SHEET
=	EQUALS	FL	FIRE LINE/FLANGE	SP	SPRINKLER
•	FOOT	FT	FOOT/FEET	SQ	SQUARE
>	GREATER THAN	гі	FOOT/FEET	SQ FT	SQUARE FEET
> "	INCH			SQ IN	SQUARE INCH
#	NUMBER	G	GAS	SS	SANITARY SEWER
%	PERCENT	GALV	GALVANIZED	SSMH	SANITARY SEWER MANHOLE
	. 2	GRND	GROUND	ST	STREET
AC	ASPHALTIC CONCRETE	GV	GATE VALVE	STA	STATION
ADD'L	ADDITIONAL			STD	STANDARD
ADJT	ADJACENT	НН	HANDHOLE	STRUCT	STRUCTURE(E, AL)
AFF	ABOVE FINISH FLOOR	HORIZ	HORIZONTAL	SW	SOUTHWEST
AP	ANGLE POINT	HT	HEIGHT	SYS	SYSTEM
APPROX	APPROXIMATE	111	HEIGHT	0.0	3.3.2m
ARCH	ARCHITECT	.=	NUEDT ELEVATION	Т	TELEPHONE OR TELEPHONE VAULT
ATB	ASPHALT TREATED BASE COURSE	IE 	INVERT ELEVATION	TBD	TO BE DETERMINED
AVE	AVENUE	IN	INCH	TBM	TEMPORARY BENCH MARK
AVE	AVENUE			TC	TOP OF CURB ELEVATION
DOD	DECIN CURD DETURN	JB, J-BOX	JUNCTION BOX	TELE	TELEPHONE
BCR	BEGIN CURB RETURN	JT	JOINT TRENCH	TEMP	TEMPORARY
BFV	BUTTERFLY VALVE	kV	KILOVOLTS	TOW	TOP OF WALL
BGS	BELOW GROUND SURFACE	kW	KILOWATT		
BLK	BLOCK(S)	KWH	KILOWATT HOURS	TP, T/P	TOP OF PIPE
BLDG	BUILDING	MAV	MANIMINA	TYP	TYPICAL
BM	BENCHMARK	MAX	MAXIMUM	UDG	UNDERGROUND
BVC	BEGIN VERTICAL CURB	MFR	MANUFACTURER		
_		MH	MANHOLE	VAP	VERTICAL ANGLE POINT
С	CONDUIT	MIN	MINIMUM, MINUTE	VC	VERTICAL CURVE
CB	CATCH BASIN	MISC	MISCELLANEOUS	VERT	VERTICAL
CF	CUBIC FEET	MON	MONUMENT IN CASE	VOL	VOLUME
CIRC	CIRCUIT, CIRCULA(R, TION)			144	WEST WINTL WINE OR WATER
CIP	CAST-IN-PLACE	N .	NORTH, NORTHING	W	WEST, WIDTH, WIDE OR WATER
CIP MON	CAST-IN-PLACE MONUMENT	N/A	NOT APPLICABLE	W/	WITH
€ C1	CENTER JOINT	NE	NORTHEAST	W/ 0	WITHOUT
Ý.	CENTER LINE	NEMA	NATIONAL ELECTRICAL MANUFACTURES	WM	WATER MAIN
CL	CROWNLINE		ASSOCIATION	WV	WATER VALVE
CLR	CLEAR	NIC	NOT IN CONTRACT		
CO	CLEANOUT	NO, No	NUMBER	XFMR	TRANSFORMER
COMM	COMMUNICATION	NTS	NOT TO SCALE		
COMPT	COMPACTED	NW	NORTHWEST		
CONC	CONCRETE				
CONST	CONSTRUCT	OC, oc	ON CENTER		
CONT	CONTINU(E, ED, OUS, ATION)	OD	OUTSIDE DIAMETER		
COORD	COORDINATE	OSHA	OCCUPATIONAL SAFETY & HEALTH		
CSBC	CRUSHED SURFACING BASE COURSE		ADMINISTRATION		
CSTC	CRUSHED SURFACING TOP COURSE				
CULV	CULVERT	Р	POWER, POWER VAULT		
CU YD	CUBIC YARD	PC	POINT OF CURVATURE		
00 10	CODIO TARD	PCC	POINT OF COMPOUND CURVE		
D/W	DRIVEWAY	PED	PEDESTAL PEDESTAL		
•	DEFLECTION	PI	POINT OF INTERSECTION		
DEF		PL	PROPERTY LINE		
DEG	DEGREE DEMOLISH /DEMOLITION	IL POC	POINT OF CONNECTION		
DEMO	DEMOLISH/DEMOLITION	PP PDC	POWER POLE		
DIA	DIAMETER DIMENSION(C)				
DIM	DIMENSION(S)	PRC	POINT OF REVERSE CURVATURE		
DIP	DUCTILE IRON PIPE	PROP	PROPERTY		
DR	DRIVE	PSI	POUNDS PER SQUARE INCH		
DWG(S)	DRAWING(S)	PT	POINT OF TANGENCY		
_		PVC	POINT OF VERTICAL CURVE		
E	EAST OR ELECTRICAL	PVI	POINT OF VERTICAL INTERSECTION		
ГА	FACU	PVT	POINT OF VERTICAL TANGENT		

POINT OF VERTICAL TANGENT

REINFORC(E, ED, ING, MENT)

STRUCTURE RIM ELEVATION

PAVEMENT

QUANTITY

RADIUS

ROAD, ROADWAY

REFERENCE

REQUIRED

REVISION

RIGHT RIGHT OF WAY

POWER

PVT

PVMT

PWR

QTY

ref Reinf

REQ'D

REV

RIM

R/W, ROW

EΑ

ECR

EHH

ELEC ENGR

E0P

EQ

ESMT

EVC

EX, EXIST

EL, ELEV

EACH

ELEVATION

EQUAL(LY)

EQUIPMENT

EASEMENT

EXISTING

EXPANSION

ELECTRIC(AL) ENGINEER

END CURB RETURN

ELECTRICAL HANDHOLE

EDGE OF PAVEMENT

END VERTICAL CURVE

REVISIONS
PRELIMINARY

SCJ PROJECT NO. 1810.35 DRAWN K. GANS CHECKED

CHECKED
J. BRUHN
SUBMITTAL DATES

1/15/21 OTB DATE

ALLIANCE

NASULTING SERVICES

OWER AVE, CENTRALIA, WA 98531

160.669.0700 F: 360.352.1509

SCJALLIANCE.COM



VINCO FOODS STORE
WW STEWART PARKWA
OSEBURG, OREGON



SHEET TITLE

GENERAL

NOTES

GN-01

SHEET

Feb 26, 2021 10:50:28am — User brandon.johnson \\CEN-FS2—SVR.AD.SCJ.IO\PROJECTS\1810 TAIT AND ASSOCIATES, INC\1810.35 WINCO FOODS ROSEBURG, OR\CADD\1810.

SUBJECT PROPERTY LEGAL DESCRIPTION

A PARCEL OF LAND LYING IN SECTIONS 11 AND 12, TOWNSHIP 27 SOUTH, RANGE 6 WEST, W.M., DOUGLAS COUNTY, OREGON, AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A 5/8" IRON ROD ON THE SOUTHEASTERLY RIGHT OF WAY LINE OF STEWART PARKWAY, SAID POINT BEING MORE PARTICULARLY DESCRIBED AS P.O.T. STATION 17+00.00 AND FROM WHICH POINT THE NORTHWEST CORNER OF DONATION LAND CLAIM NO. 65 BEARS SOUTH 43" 35' 24" WEST 3615.62 FEET; THENCE ALONG SAID SOUTHEASTERLY RIGHT OF WAY LINE SOUTH 81 52 57" WEST 46.60 FEET TO THE TRUE POINT OF BEGINNING, SAID POINT BEING THE NORTHWEST CORNER OF THAT PROPERTY DESCRIBED IN INSTRUMENT NO. 68-9521 IN THE DEED RECORDS OF DOUGLAS COUNTY, OREGON; THENCE ALONG THE WEST LINE OF SAID PROPERTY DESCRIBED IN INSTRUMENT NO. 68-9521, SOUTH 0' 04' 42" WEST 512.51 FEET TO A 5/8 INCH IRON ROD; THENCE CONTINUING SOUTH 0' 04' 42" WEST 83.58 FEET; THENCE NORTH 75' 18' 40" WEST 30.93 FEET; THENCE SOUTH 41' 47' 08" WEST 105.93 FEET; THENCE SOUTH 75' 18' 21" WEST 216.15 FEET; THENCE SOUTH 31' 10' 50" WEST 65.0 FEET; THENCE NORTH 42' 10' 11" WEST 714.25 FEET TO A 5/8 INCH IRON ROD ON THE ABOVE SAID SOUTHEASTERLY RIGHT OF WAY LINE; THENCE ALONG SAID SOUTHEASTERLY RIGHT OF WAY LINE ALONG THE ARC OF A 676.20 FOOT RADIUS CURVE TO THE RIGHT (THE LONG CHORD OF WHICH BEARS NORTH 63' 50' 09" EAST 418.09 FEET) 425.05 FEET AND NORTH 81° 52' 57" EAST 452.84 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM THE NORTHERLY FOUR FEET OF THE PROPERTY; SAID FOUR FEET BEING ADJACENT AND SOUTHERLY OF THE PRESENT STEWART PARKWAY RIGHT OF WAY, AND DESCRIBED IN DEED TO CITY OF ROSEBURG, RECORDED NOVEMBER 7, 1990 IN BOOK 1117, PAGE 936, RECORDER'S NO. 90—16515, RECORDS OF DOUGLAS COUNTY, OREGON.

"AS SURVEYED" PROPERTY LEGAL DESCRIPTION

A PARCEL OF LAND LYING IN THE NORTHEAST QUARTER (NE 1/4) OF SECTION 11 AND THE THE NORTHWEST QUARTER (NW 1/4) OF SECTION 12, TOWNSHIP 27 SOUTH, RANGE 6 WEST, W.M., DOUGLAS COUNTY, OREGON; SAID TRACT BEING ALL OF THAT TRACT DESCRIBED IN INSTRUMENT NUMBER 1990—16516, DOUGLAS COUNTY CLERK'S RECORDS AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT A 5/8" IRON ROD ON THE SOUTHEASTERLY RIGHT OF WAY LINE OF STEWART PARKWAY, SAID POINT ALSO BEING THE NORTHWEST CORNER OF PARCEL 1, PARTITION PLAT NUMBER 2000-0031, DOUGLAS COUNTY SURVEY RECORDS; THENCE ALONG THE EAST LINE OF SAID INSTRUMENT NUMBER 1990-16516, SOUTH 00'07'45" WEST, 593.01 FEET TO A 5/8" IRON ROD MARKING THE SOUTHEAST CORNER OF SAID INSTRUMENT NUMBER 1990-16516; THENCE ALONG THE SOUTH BOUNDARY OF SAID INSTRUMENT NUMBER 1990-16516 THE FOLLOWING COURSES:

NORTH 76'54'38" WEST, 30.91 FEET TO A POINT; THENCE SOUTH 42'05'07" WEST, 105.92 FEET TO A 5/8" IRON ROD; THENCE

SOUTH 42'05'07" WEST, 105.92 FEET TO A POINT; THENCE
SOUTH 75'30'38" WEST, 216.26 FEET TO A POINT; THENCE
SOUTH 30'14'50" WEST, 64.99 FEET TO A 5/8" IRON ROD MARKING THE SOUTHWEST CORNER OF SAID INSTRUMENT
NUMBER 1990—16516; THENCE ALONG THE WEST LINE OF SAID INSTRUMENT NUMBER 1990—16516, NORTH
42'21'31" WEST, 710.12 FEET TO A POINT ON SAID SOUTHEASTERLY RIGHT—OF—WAY OF STEWART PARKWAY; THENCE
ALONG SAID SOUTHEASTERLY RIGHT—OF—WAY OF STEWART PARKWAY, ALONG A 672.20—FOOT RADIUS CURVE TO THE RIGHT, THE LONG CHORD OF WHICH BEARS NORTH 63 40 36" EAST, 415.91 FEET, AN ARC DISTANCE OF 422.84 FEET TO A 2" BRASS DISC; THENCE CONTINUING ALONG SAID SOUTHEASTERLY RIGHT—OF—WAY OF STEWART PARKWAY, NORTH 81'41'49" EAST, 455.00 FEET TO THE POINT OF BEGINNING AND CONTAINING 9.47 ACRES, MORE OR LESS.

ALTA COMMITMENT FOR TITLE INSURANCE

FIRST AMERICAN TITLE INSURANCE COMPANY NATIONAL ISSUING OFFICE COMMITMENT NO.: NCS-987887B-OR1

THE FOLLOWING SURVEY RELATED MATTERS LISTED IN THE SPECIAL EXCEPTIONS PORTION OF THE ABOVE REFERENCED TITLE REPORT AFFECT THE SUBJECT PROPERTY

15. Covenants, conditions, restrictions and/or easements; but deleting any covenant, condition or restriction indicating a preference, limitation or discrimination based on race, color, religion, sex, handicap, family status, or national origin to the extent such covenants, conditions or restrictions violate Title 42, Section 3604(c), of the United States Codes: Recording Information: Book 547, Page 815, Recorder's No. 74-7577

Modification and/or amendment by instrument: Recording Information: Book 856, Page 533, Recorder's No. 83—9969

SURVEYOR'S NOTE: EASEMENT REFERENCED IN DOCUMENT NOT LOCATED ON SUBJECT PROPERTY 16. Easement, including terms and provisions contained therein:

Recording Information: Book 612, Page 134, Recorder's No. 76–15765
In Favor of: Pacific Power and Light Company
For: Right of Way (Exact location not disclosed)

SURVEYOR'S NOTE: UNABLE TO SHOW EASEMENT. EXACT LOCATION NOT DISCLOSED IN DOCUMENT.

17. Easement, including terms and provisions contained therein: Recording Information: Book 848, Page 262, Recorder's No. 83-5692 In Favor of: CP National Corporation Natural Gas line, 10 feet in width (Exact location not disclosed)

SURVEYOR'S NOTE: UNABLE TO SHOW EASEMENT. EXACT LOCATION NOT DISCLOSED IN DOCUMENT (BLANKET EASEMENT FOR ALL UNDERGROUND GAS LINES AND APPURTENANCES)

19. Easement, including terms and provisions contained therein: Recording Information: Book 856, Page 256, Recorder's No. 83-9825 In Favor of: Pacific Power and Light Company

SURVEYOR'S NOTE: EXACT LOCATION OF EASEMENT NOT DISCLOSED IN DOCUMENT. ALL UNDERGROUND POWER LOCATED PER ONE—CALL SHOWN ON THIS SURVEY. 21. Easement, including terms and provisions contained therein:

Recording Information: Book 866, Page 467, Recorder's No. 83-14957 City of Roseburg Water line, 10 feet in width

SURVEYOR'S NOTE: SHOWN ON PAGE 2 OF THIS SURVEY

23. Easement, including terms and provisions contained therein: Recording Information: Book 1249, Page 368, Recorder's No. 93-16146
In Favor of: City of Roseburg
For: Water line, 15 feet in width

SURVEYOR'S NOTE: SHOWN ON PAGE 2 OF THIS SURVEY

25. Easement, including terms and provisions contained therein: Recording Information: Book 1648, Page 668, Recorder's No. 99–24784 In Favor of: PacifiCorp In Favor of: Right of Way SURVEYOR'S NOTE: 10' EASEMENT CENTERED OVER EXISTING UNDERGROUND POWER, AS LOCATED IN FIELD. EASEMENT SHOWN ON THIS SURVEY DOES NOT MATCH INFORMATION SHOWN IN EXHIBIT ATTACHED TO DOCUMENT.

SURVEYOR'S CERTIFICATE

The undersigned hereby certifies to WinCo Foods, LLC and First American Title Insurance Company, that this map or plat and the survey on which it is based were made in accordance with the "2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys", jointly established and adopted by ALTA and NSPS, and includes "MSDR" Items 1, 2, 3, 4, 5, 6b, 7a, 8, 9, 10a, 11, 13, 14, 16, 17, 18, 19 and 20 of Table A thereof. The fieldwork was completed on July ??, 2020.

PRELIMINARY

ALTA/ACSM LAND TITLE SURVEY FOR

WINCO FOODS, LLC SITUATED IN THE NORTHEAST QUARTER OF SECTION 11 AND THE NORTHWEST QUARTER OF SECTION 12, TOWNSHIP 27 SOUTH, RANGE 6 WEST, WILLAMETTE MERIDIAN, CITY OF ROSEBURG, DOUGLAS COUNTY, OREGON

SANITARY AND STORM SEWER RIM & INVERT ELEVATIONS

- ① EXISTING SANITARY SEWER MANHOLE RIM = 479.76 8" INV. IN SW = 474.41 10" INV. IN SE = 474.71 10" INV. OUT NW = 474.26
- ② EXISTING BOX CULVERT FL = 469.83
- 3 EXISTING STS MH RIM = 482.03 12" INV. IN NW = 478.08 12" INV. IN NE = 478.02 12" OUT SW = 477.74
- EXISTING CURB INLET
 RIM = 482.51
- 12" INV. OUT NW = 477.87 ⑤ EXISTING STS MH RIM = 482.23 12" INV. IN SE = 477.97 12" INV. IN NE = 478.10

12" INV. OUT SW = 477.94

- 6 EXISTING STS MH RIM = 483.36 12" INV. IN N = 479.52
- 12" INV. OUT SW = 479.47EXISTING CURB INLET RIM = 483.54
- 12" INV. OUT SE = 479.76(8) EXISTING OIL SEPARATOR CATCH BASIN GRATE = 485.68
- 6" INV. OUT S = 484.31
- EXISTING CATCH BASIN
 GRATE = 481.80
 10" INV. IN E = 478.82 12" INV. OUT S = 478.82
- EXISTING CATCH BASIN GRATE = 483.76
- 6" INV. OUT E = 482.3611) EXISTING CATCH BASIN GRATE = 481.33 12" INV. IN N = 477.20 12" INV. OUT S = 477.20
- 12 EXISTING 12" ADS (OUTFALL) INV. = 474.61
- 13) EXISTING STS VAULT MH RIM = 477.80 60" CMP INV. N = 469.1460" CMP INV. NE = 477.84
- EXISTING STS VAULT MH RIM = 478.03 BOX CULVERT INV. = 468.59
- EXISTING CATCH BASIN GRATE = 477.66 12" INV. OUT S = 475.57
- 16 EXISTING CATCH BASIN GRATE = 478.85 4" INV. IN SE = 477.17 6" INV. IN SF = 476.64 6" INV. OUT NW = 475.98
- 17 EXISTING SANITARY SEWER MANHOLE RIM = 477.58
 10" INV. IN NW = 471.03
 12" INV. IN NE = 470.95 10" INV. IN SE = 471.0012" INV. OUT SW = 470.86
- (18) EXISTING SANITARY SEWER MANHOLE RIM = 481.47 8" INV. IN NE = 476.76
- 8" OUT NW = 476.62 19 EXISTING STS MANHOLE 18" INV. IN NE = 472.97
- 18" OUT NW = 473.0720 EXISTING CURB INLET
- 12" INV. OUT SE = 478.8021) EXISTING CURB INLET
- RIM = 479.18
- 22) EXISTING BOX CULVERT INV. = 469.58 23 EXISTING CURB INLET RIM = 482.25
- 12" INV. OUT S = 478.0924 EXISTING 48" CMP (OUTFALL)
- 48" INV. OUT SE = 471.90 25 EXISTING 48" CMP (OUTFALL)
- 26 EXISTING 48" CMP (OUTFALL) 48" INV. OUT SE = 472.71
- 27 EXISTING CATCH BASIN GRATE = 479.47 12" INV. OUT SE = 477.35
- 28) EXISTING CATCH BASIN GRATE = 478.58

FL = 470.34

BUILDING SETBACK REQUIREMENTS

THE FOLLOWING SETBACK REQUIREMENTS ARE LISTED PER ROSEBURG COMMUNITY DEVELOPMENT DEPARTMENT LAND USE DEVELOPMENT ORDINANCE, PER SAID ORDINANCE, THE SETBACK REQUIREMENTS FOR ANY TRACT ABUTTING A TRACT ZONED PR (PUBLIC RESERVE) SHALL MAINTAIN REAR AND SIDE SETBACKS AS REQUIRED FOR PUBLIC RESERVE ZONING.

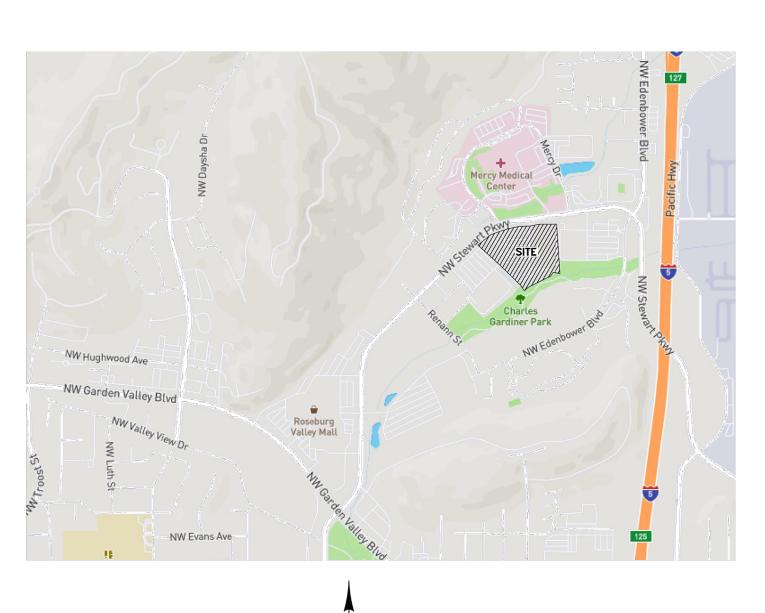
SIDE (INTERIOR): 5-FEET SIDE (EXTERIOR): 20-FEET

GENERAL NOTES

- SUBJECT PROPERTY AND ABUTTING PROPERTY ARE ZONED C3 (GENERAL COMMERCIAL), WITH THE EXCEPTION OF THE TRACT LOCATED DIRECTLY SOUTH, WHICH IS OWNED BY THE CITY OF ROSEBURG AND ZONED PR (PUBLIC
- BUILDING HEIGHT SHALL NOT EXCEED 80' PER CITY OF ROSEBURG COMMUNITY DEVELOPMENT DEPARTMENT LAND USE DEVELOPMENT ORDINANCE FOR GENERAL COMMERCIAL ZONING
- 3. THERE IS A DISCREPANCY IN GROSS LAND AREA BETWEEN THIS SURVEY AND THE DOUGLAS COUNTY ASSESSOR'S DATA. THE SUBJECT PROPERTY, AS SURVEYED ON THIS MAP CONTAINS 9.47 ACRES, MORE OR LESS. DOUGLAS COUNTY ASSESSOR'S OFFICE RECORDS INDICATE THE ACREAGE TO BE 9.45

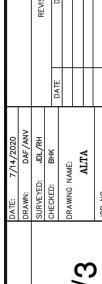
FLOOD ZONE CERTIFICATION

A PORTION OF THE SUBJECT PROPERTY LIES WITHIN FLOOD ZONE "AE" IDENTIFIED ON F.I.R.M. MAP NUMBER 41019C1726F, DATED FEBRUARY 17, 2010. APPROXIMATE BOUNDARY OF ZONE "AE" PER SAID MAP SHOWN ON PAGE 3 OF THIS SURVEY. ABOVE-REFERENCED F.I.R.M. MAP REFERENCED TO NAVD 88 VERTICAL DATUM.



VICINITY MAP

(NOT TO SCALE)





REGISTERED PROFESSIONAL LAND SURVEYOR

PRELIMINARY

DEREK ALLEN FEIGEL 77555

EXPIRES: 12/31/21

ORE SKW, ON



REVISIONS

PRELIMINARY

CJ PROJECT NO.

1810.35

DRAWN K. GANS

HECKED J. BRUHN

/15/21

B DATE

REFERENCE

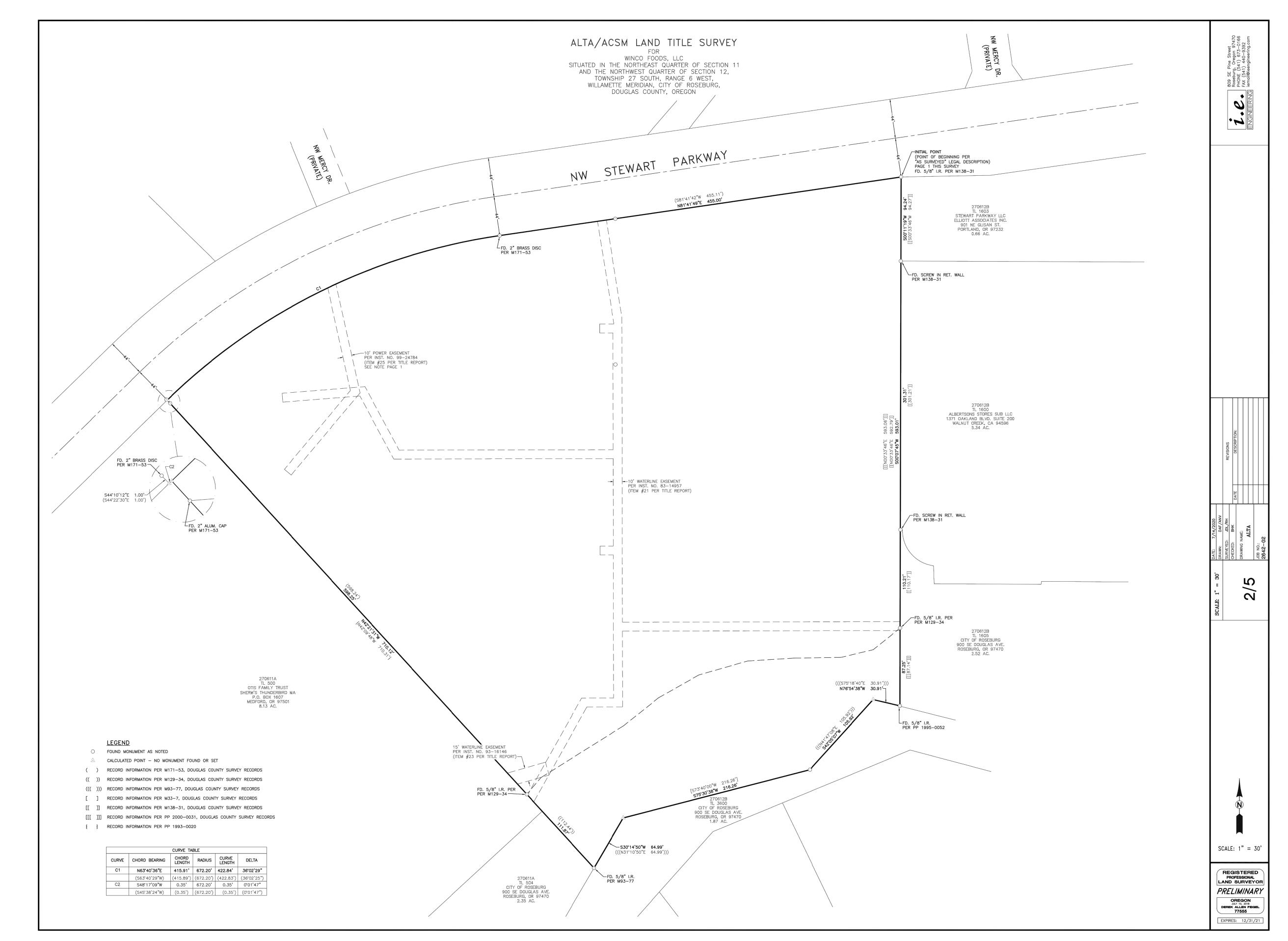
SHEET TITLE

ALTA SURVEY

SV-01

SHEET

DEREK ALLEN FEIGEL OREGON PLS NUMBER: 77555 DATE: 7/??/2020

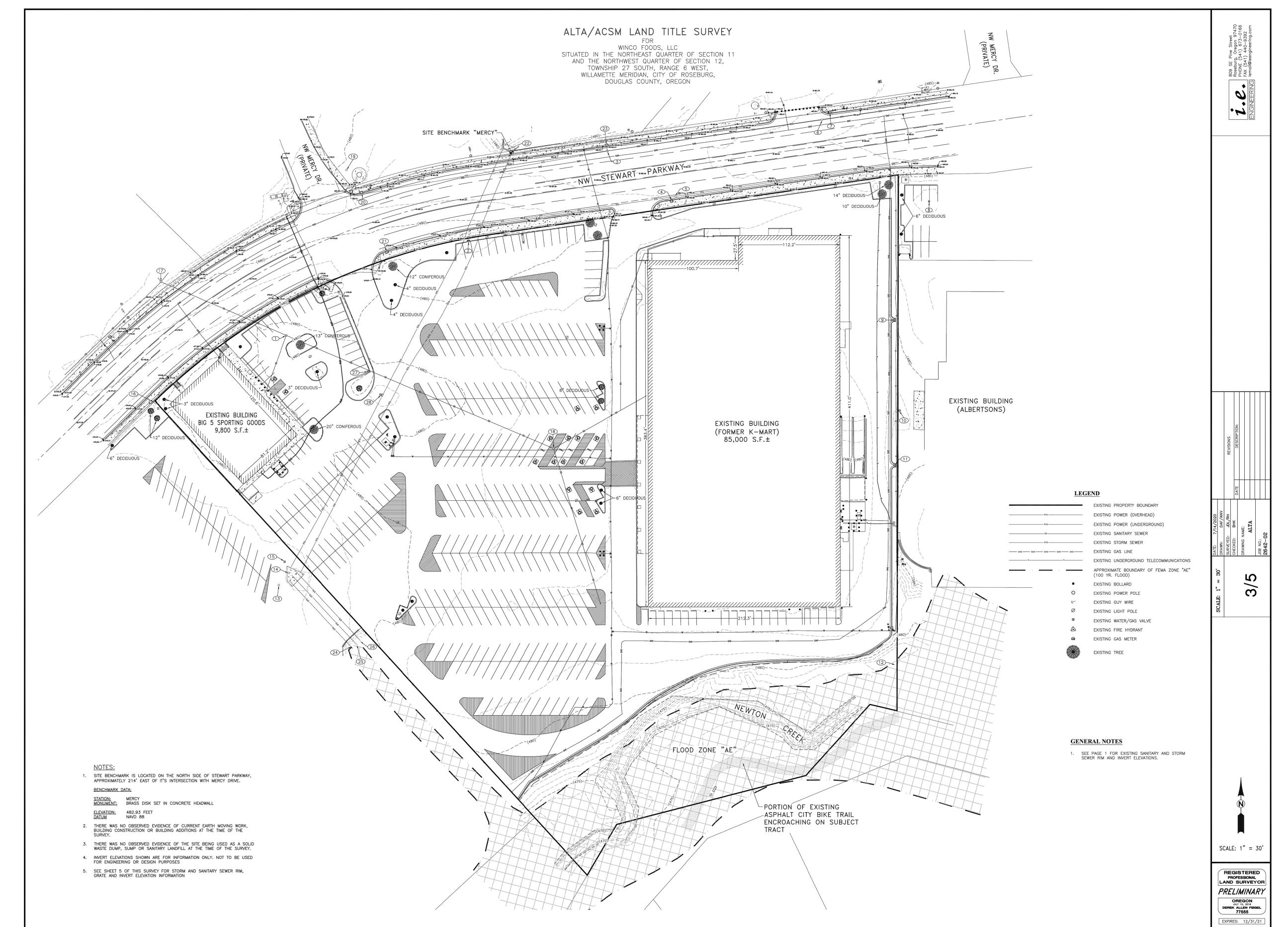


REVISIONS PRELIMINARY 1810.35 K. GANS CHECKED J. BRUHN 1/15/21 REFERENCE WINCO FOODS STORE NW STEWART PARKWAY ROSEBURG, OREGON

ALTA SURVEY

SHEET TITLE

SV-02

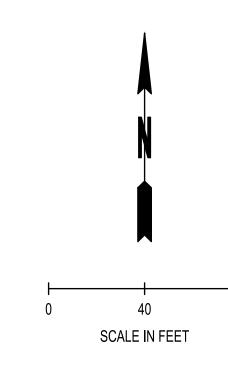


PRELIMINARY K. GANS J. BRUHN 1/15/21 REFERENCE

SHEET TITLE

ALTA SURVEY

SV-03



EXISTING CONTOURS (MAJOR/MINOR) — — — — — SEDIMENT FENCE

STRAW WATTLE

INLET PROTECTION PROPOSED DRAINAGE PATTERN

EXISTING DRAINAGE PATTERN

GENERAL NOTES:

- 1. A GENERAL PERMIT NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) STORMWATER DISCHARGE PERMIT SHALL BE OBTAINED PRIOR TO IN ACCORDANCE TO THE REQUIREMENTS OF THE NPDES PERMIT THROUGHOUT
- BMPS SHOWN ON THIS PLAN ARE THE MINIMUM BMPS REQUIRED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THE SITE IS COMPLIANT AT ALL TIMES AND INSTALL ADDITIONAL BMPS AS NECESSARY.
- 3. CONTRACTOR SHALL SECURE ALL STORAGE AREAS.
- 4. CONTRACTOR SHALL PROVIDE ALL EROSION CONTROL MEASURES REQUIRED FOR STORAGE AND STOCKPILE AREAS.
- 5. ALL MATERIAL STORED ON SITE SHALL HAVE PROPER ENCLOSURES AND/OR
- 6. CONTRACTOR SHALL PROVIDE DESIGNATED PAINT AND WASTE DISPOSAL LOCATION AS NECESSARY.
- 7. CONTRACTOR SHALL PROVIDE CONCRETE WASHOUT AREA.
- 8. SEE LANDSCAPE PLANS FOR SEEDING AND PLANTINGS.
- 9. CONTRACTOR SHALL INSTALL SILT FENCE, CONSTRUCTION ENTRANCE AND INLET PROTECTION IN EXISTING INLETS PRIOR TO COMMENCING ANY SITE DISTURBING ACTIVITIES.
- 10. CONTRACTOR SHALL PROVIDE EROSION CONTROL MEASURE FOR ALL SLOPES DURING AND AFTER CONSTRUCTION PER THE TABLE BELOW: <u>SLOPE</u>

5:1-4:1 (20%-25%) TENSAR ROLLMAX S150BN

4:1-2:1 (25%-50% TENSAR ROLLMAX SC150BN

2:1-1:1 (50-100%) TENSAR ROLLMAX C125BN

PRELIMINARY

K. GANS J. BRUHN

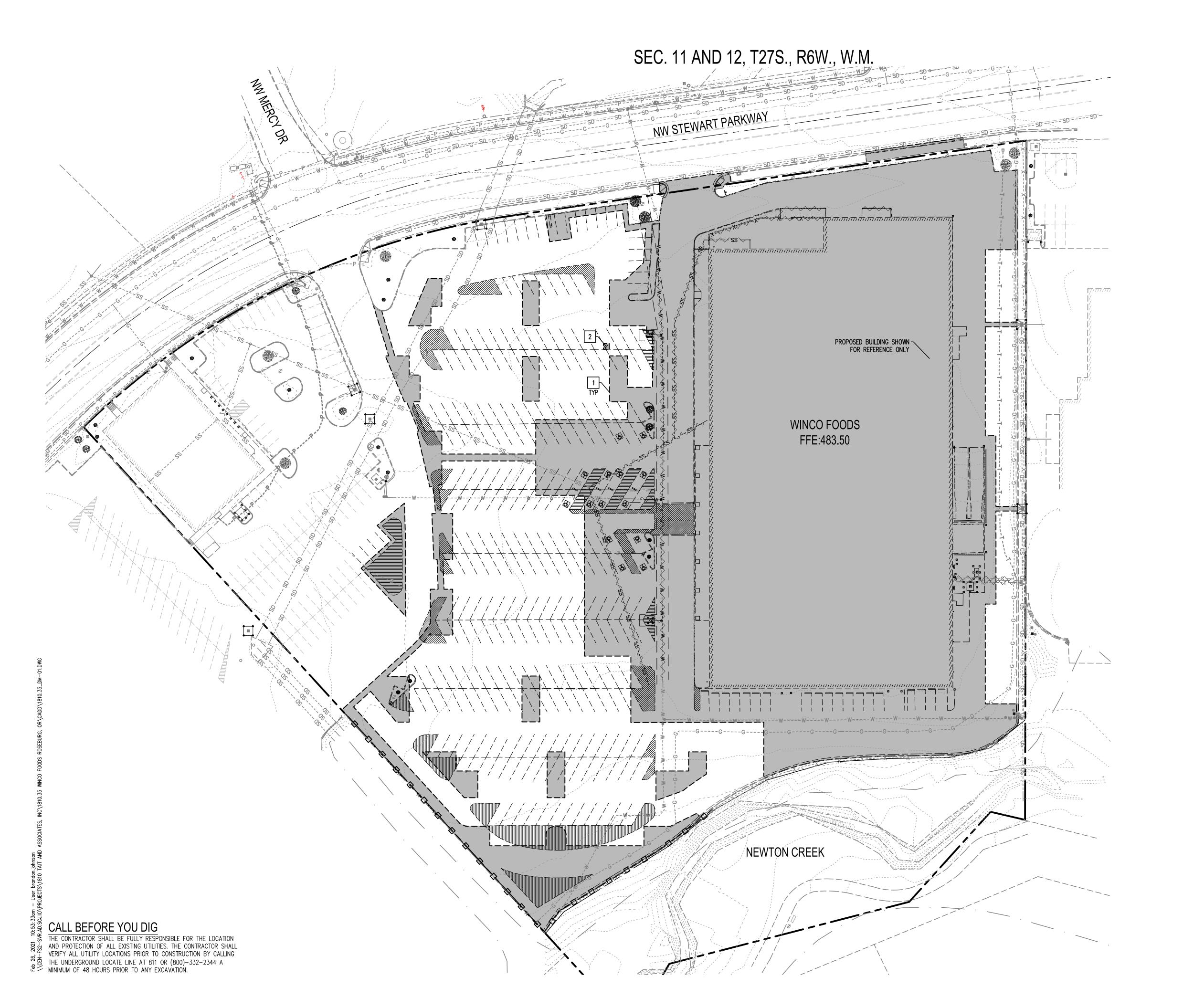


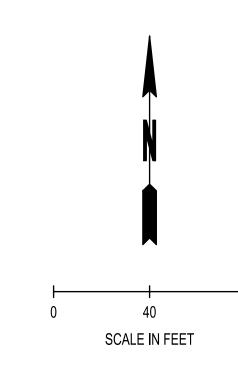


SHEET TITLE **EROSION**

CONTROLPLAN

SHEET EC-01





EXISTING ASPHALT/CONCRETE/SIDEWALK/CURB AND BASE TO BE REMOVED

REMOVE EXISTING FENCE/GATE AND BACKFILL

REMOVE EXISTING PAINT STRIPE EXISTING LIGHT STANDARD

EXISTING GUY-WIRE

REMOVE EXISTING WATER LINE AND BACKFILL

REMOVE EXISTING TELEPHONE LINE AND BACKFILL

REMOVE EXISTING GAS LINE AND BACKFILL

GENERAL DEMOLITION NOTES:

- EXISTING IMPROVEMENTS ARE SHOWN TO THE EXTENT OF ENGINEER'S
- AWARDED CONTRACTOR WILL BE PROVIDED AUTOCAD FILE TO LOCATE AND STAKE DEMO LIMITS.

DEMOLITION NOTES:

SAWCUT

2. REMOVE LIGHT AND FOUNDATION

PRELIMINARY

K. GANS

J. BRUHN

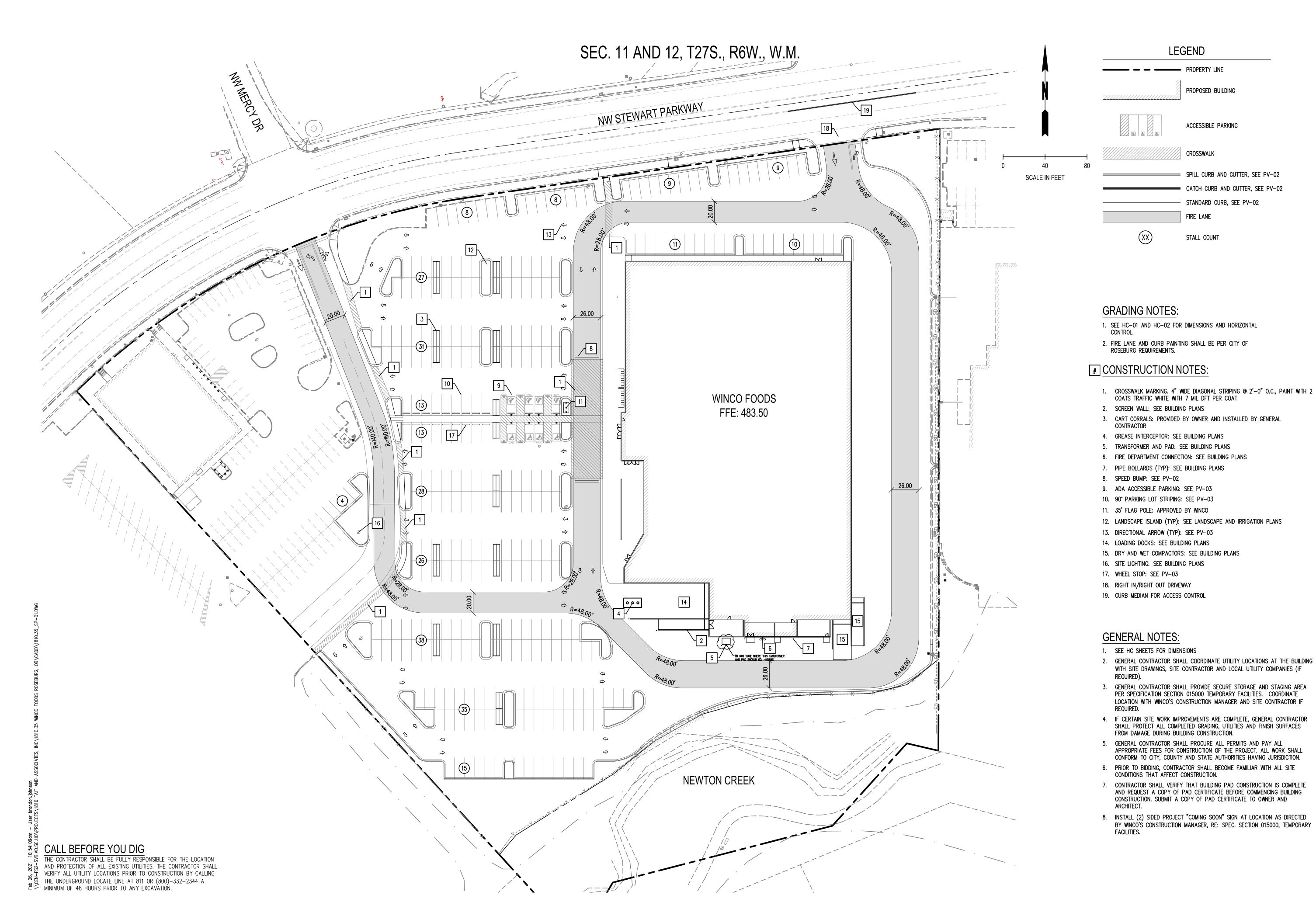




DEMOLITION

PLAN

DM-01



PRELIMINARY

LEGEND

- PROPERTY LINE

PROPOSED BUILDING

ACCESSIBLE PARKING

SPILL CURB AND GUTTER, SEE PV-02

STANDARD CURB, SEE PV-02

CROSSWALK

FIRE LANE

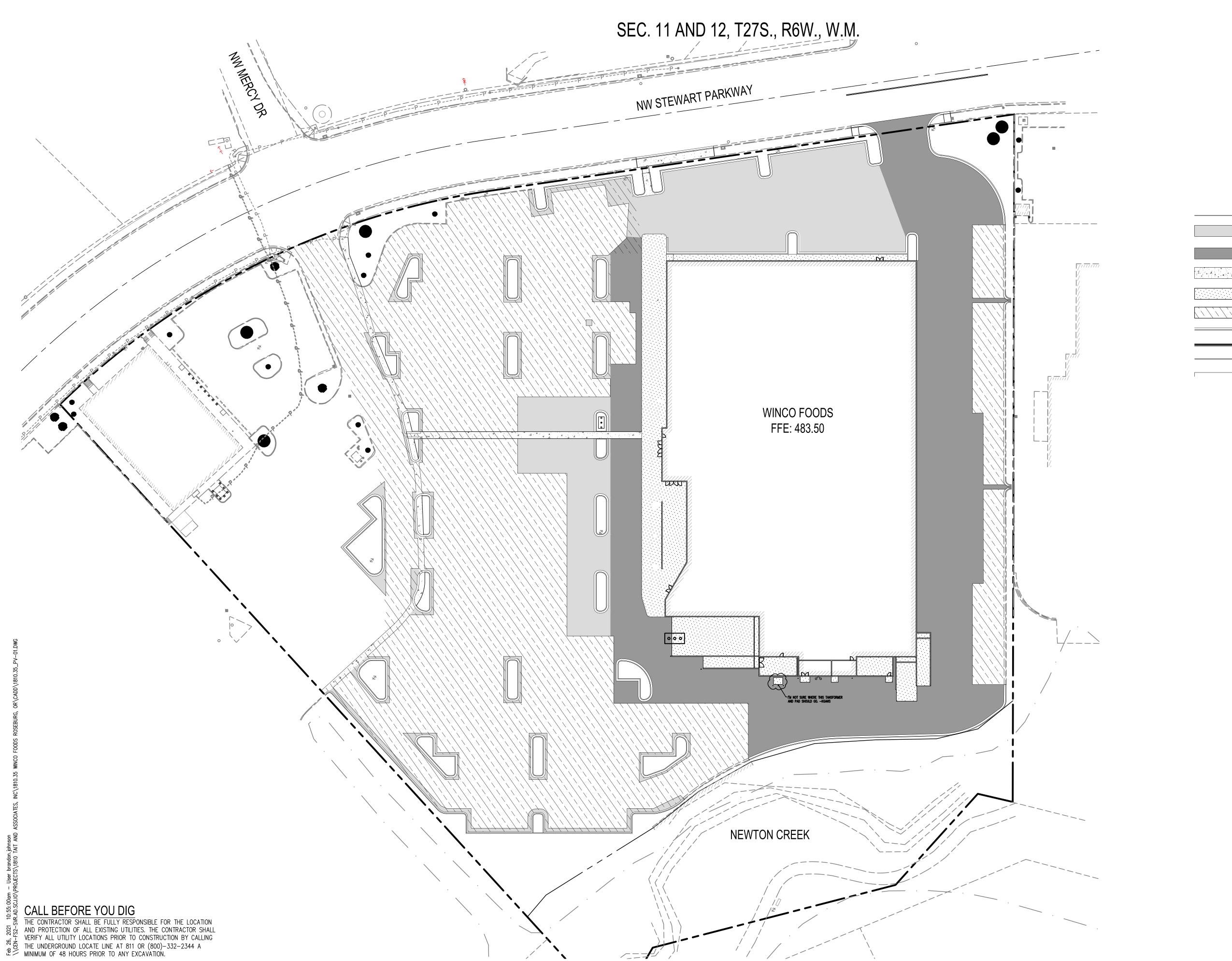
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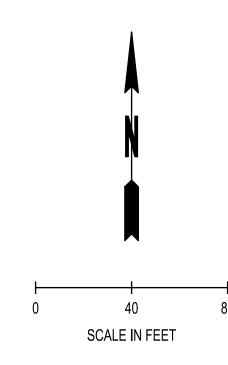
K. GANS J. BRUHN

SHEET TITLE

SHEET

SITEPLAN





LIGHT DUTY ASPHALT PAVING SEE PV-02 HEAVY DUTY ASPHALT PAVING SEE PV-02 CONCRETE SIDEWALK - ONSITE, SEE PV-02 CONCRETE PAVING WITHIN BUILDING/SITE LINE SEE BUILDING PLANS SLURRY SEAL TYPE II ■ SPILL CURB AND GUTTER, SEE PV-02

STANDARD CURB, SEE PV-02

EDGE OF PAVEMENT

PRELIMINARY

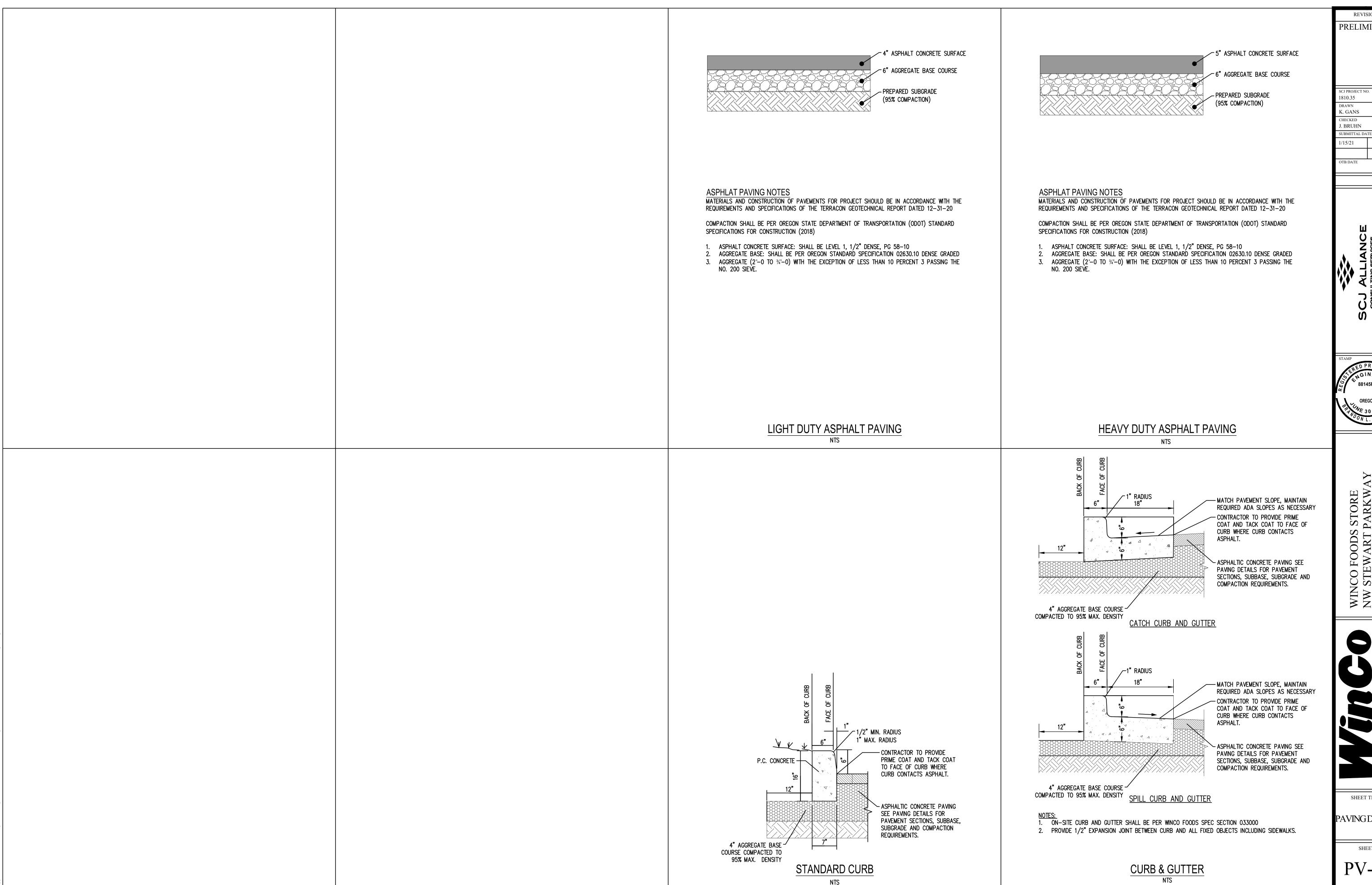
DRAWN K. GANS

J. BRUHN



PAVINGPLAN

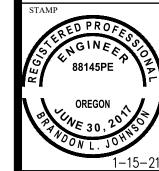
PV-01



REVISIONS **PRELIMINARY**

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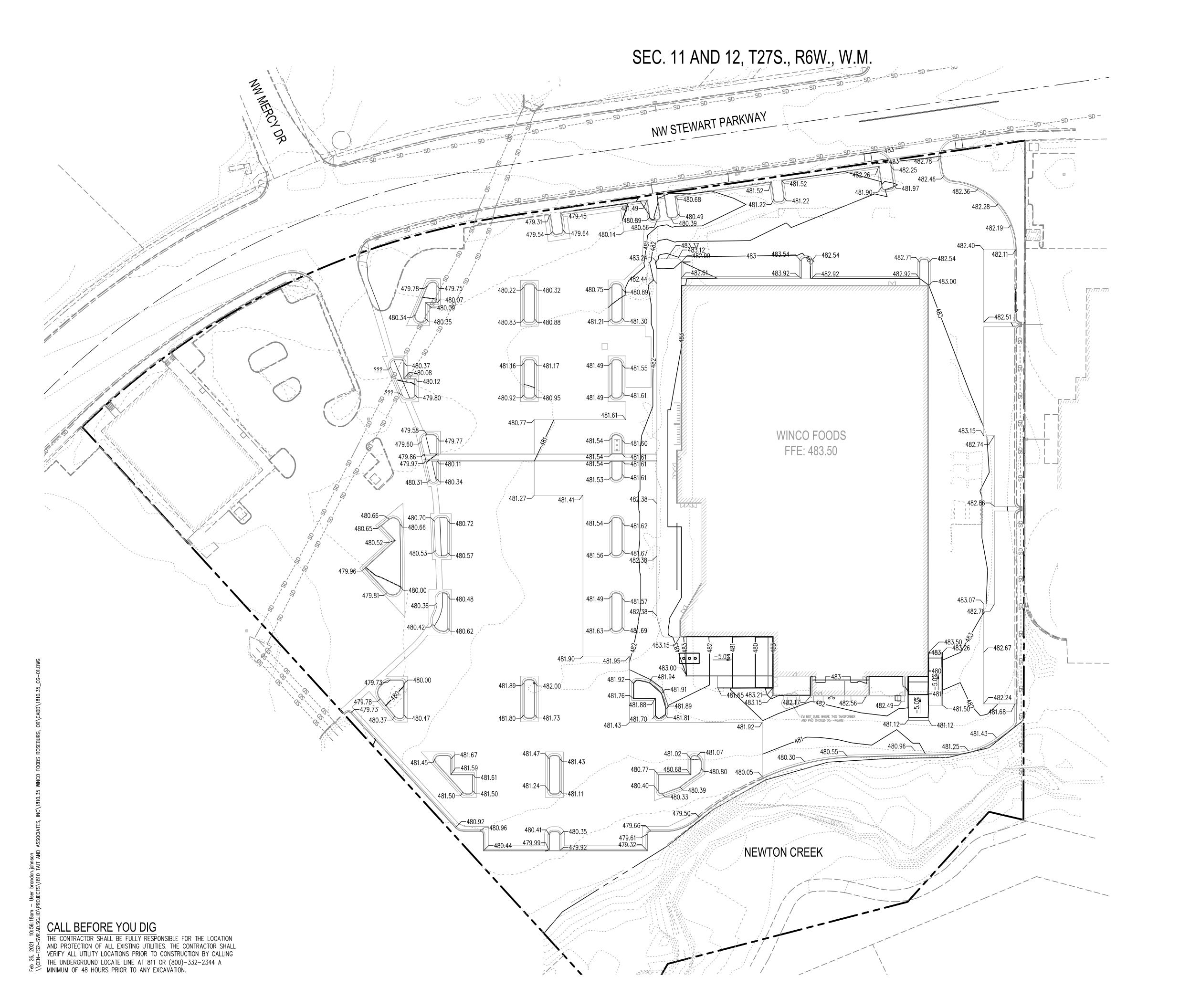
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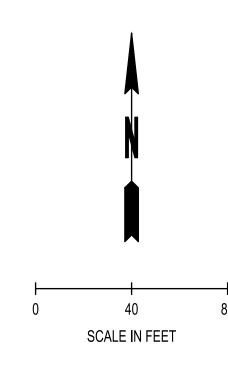


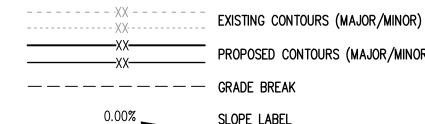
SHEET TITLE

PAVING DETAIL

PV-02







GRADING NOTES:

- 1. SEE "SD" SHEETS FOR STORM WATER INLET, PIPE, AND DETENTION SYSTEM
- 2. CONTRACTOR SHALL ENSURE THERE IS POSITIVE DRAINAGE AWAY FROM BUILDING
- AT ALL TIMES.

 3. EXISTING CONTOURS ARE BASED ON TOPOGRAPHIC SURVEY BY I.E. ENGINEERING.
- 4. SPOT ELEVATIONS REPRESENT FINISHED GRADE AT TOP OF PAVEMENT UNLESS OTHERWISE NOTED.
- 5. ALL LANDSCAPE AREAS SHALL BE STABILIZED.

PRELIMINARY

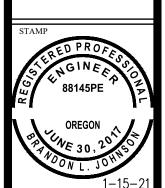
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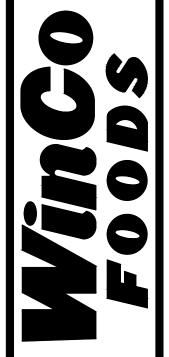
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L. DRIJINI

J. BRUHN
SUBMITTAL DATES
1/15/21
OTB DATE

CONSULTING SERVICES
N TOWER AVE, CENTRALIA, WA 98531
S: 360.669.0700 F: 360.352.1509
SCIALLIANCE.COM



WINCO FOODS STORE NW STEWART PARKW ROSEBURG, OREGON



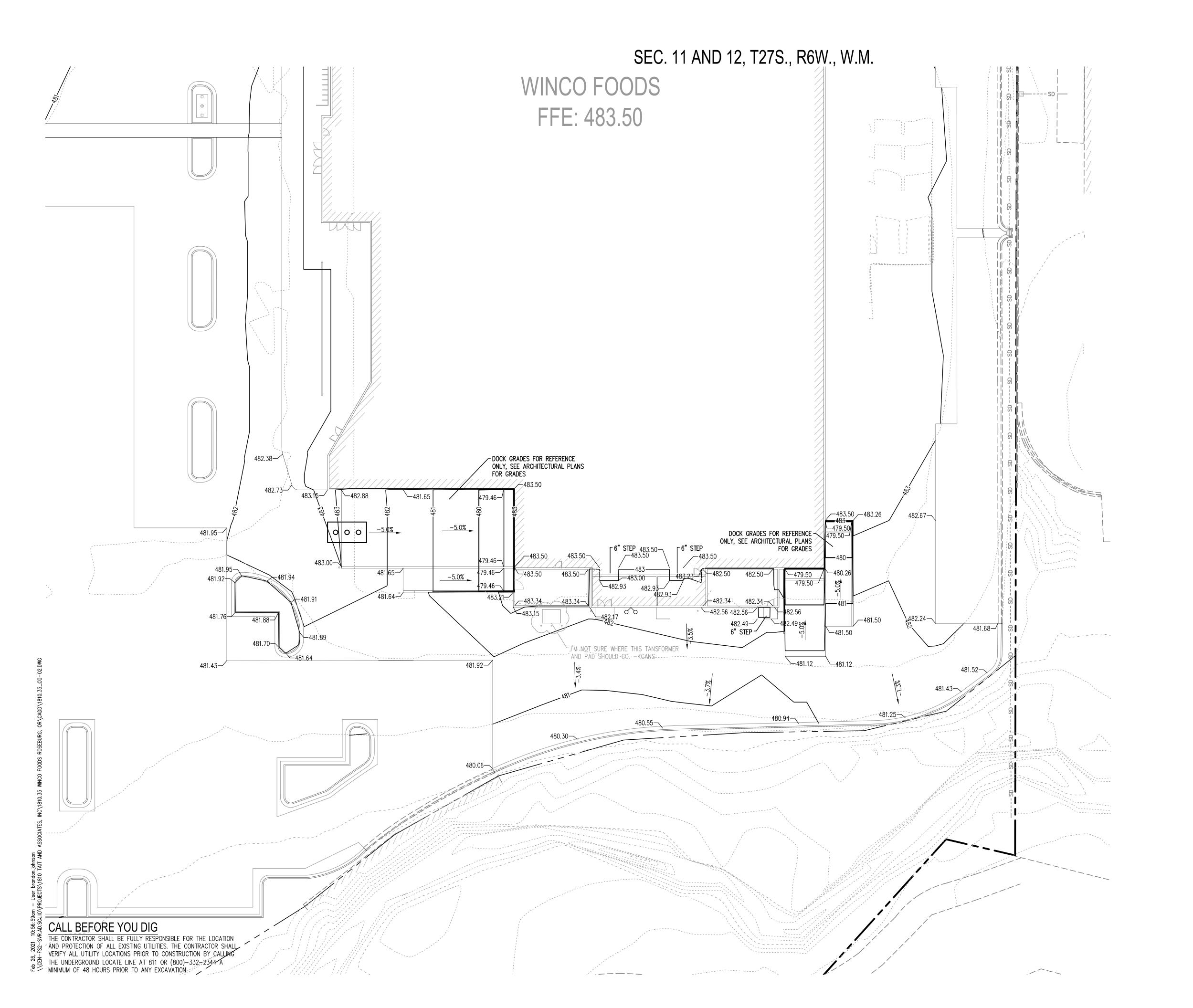
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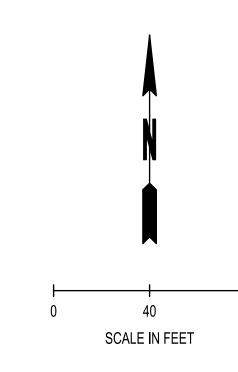
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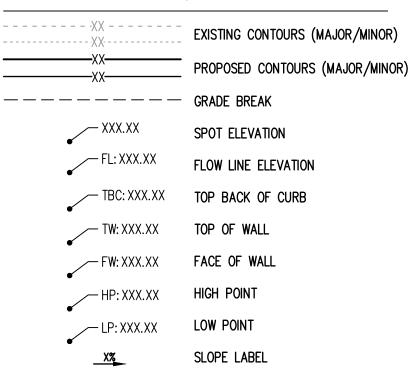
GRADING PLAN

SHEET

CG-01







GRADING NOTES:

- 1. SEE "SD" SHEETS FOR STORM WATER INLET, PIPE, AND DETENTION SYSTEM
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- 5. ALL LANDSCAPE AREAS SHALL BE STABILIZED.

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SCJ PROJECT NO.

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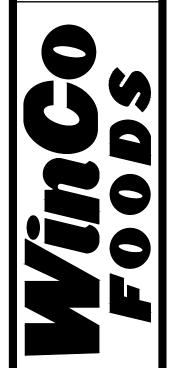
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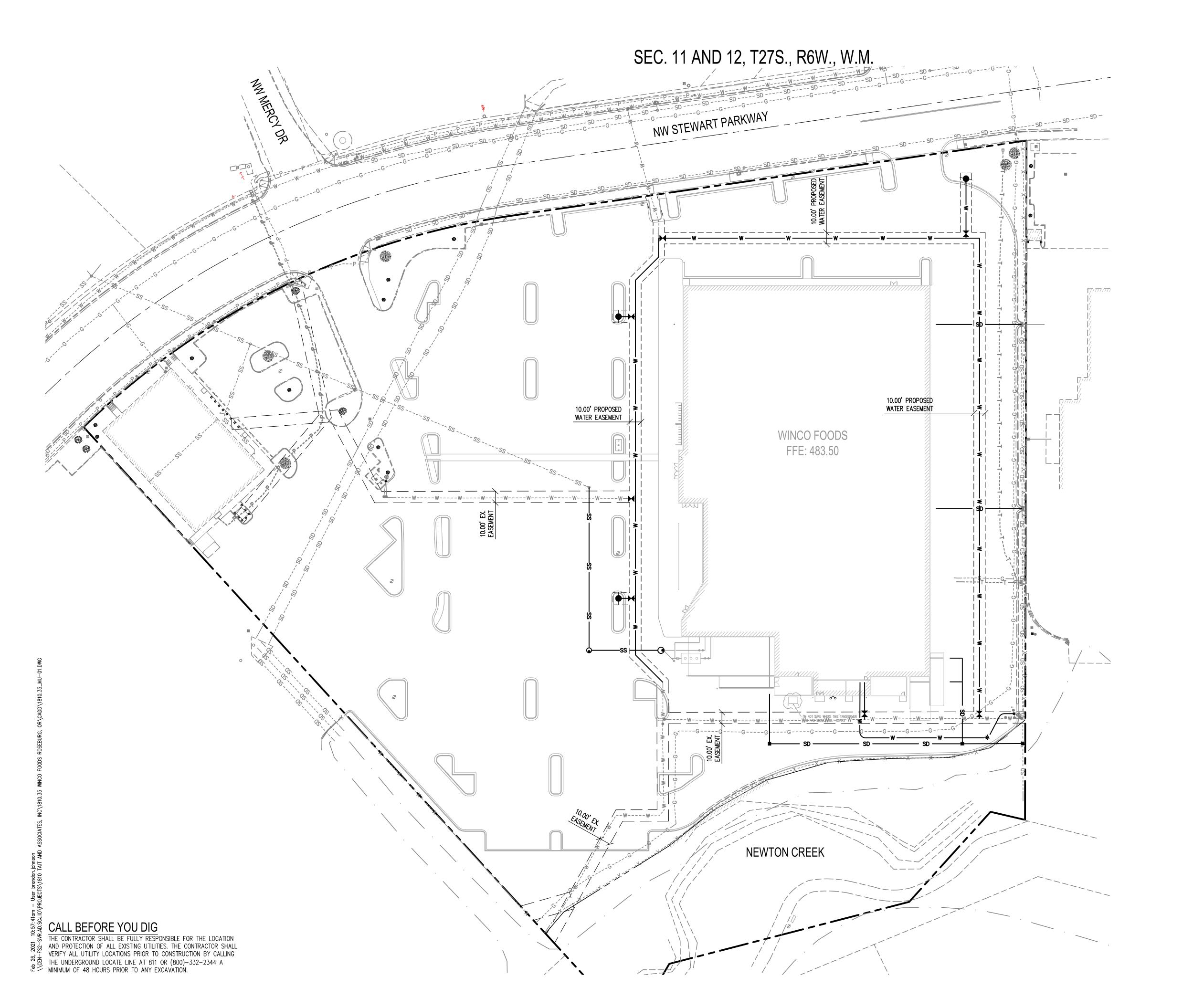
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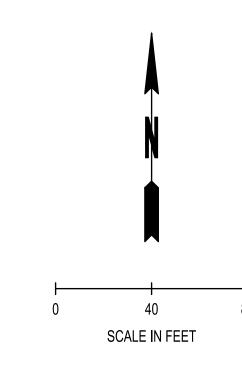
DETAILED

GRADING PLAN

SHEET

CG-02





SD	PROPOSED ADS N-12 STORM F
SDSDSD -	EXISTING STORM DRAIN LINE
ss	PROPOSED SANITARY SEWER LI
- SS SS	EXISTING SANITARY SEWER LINE
———w——	PROPOSED WATER LINE
	FXISTING WATER LINE

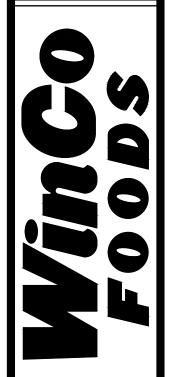
GENERAL NOTES:

1. UTILITIES SHOWN ARE FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE WITH UTILITY PROVIDERS (POWER, PHONE, GAS, CABLE, AND COMM.) ON LOCATION. CONTRACTOR SHALL REVIEW LOCATION WITH UTILITY PROVIDER AND ADJUST UTILITY LOCATION TO AVOID CONFLICTS W/ON-SITE UTILITIES.

PRELIMINARY

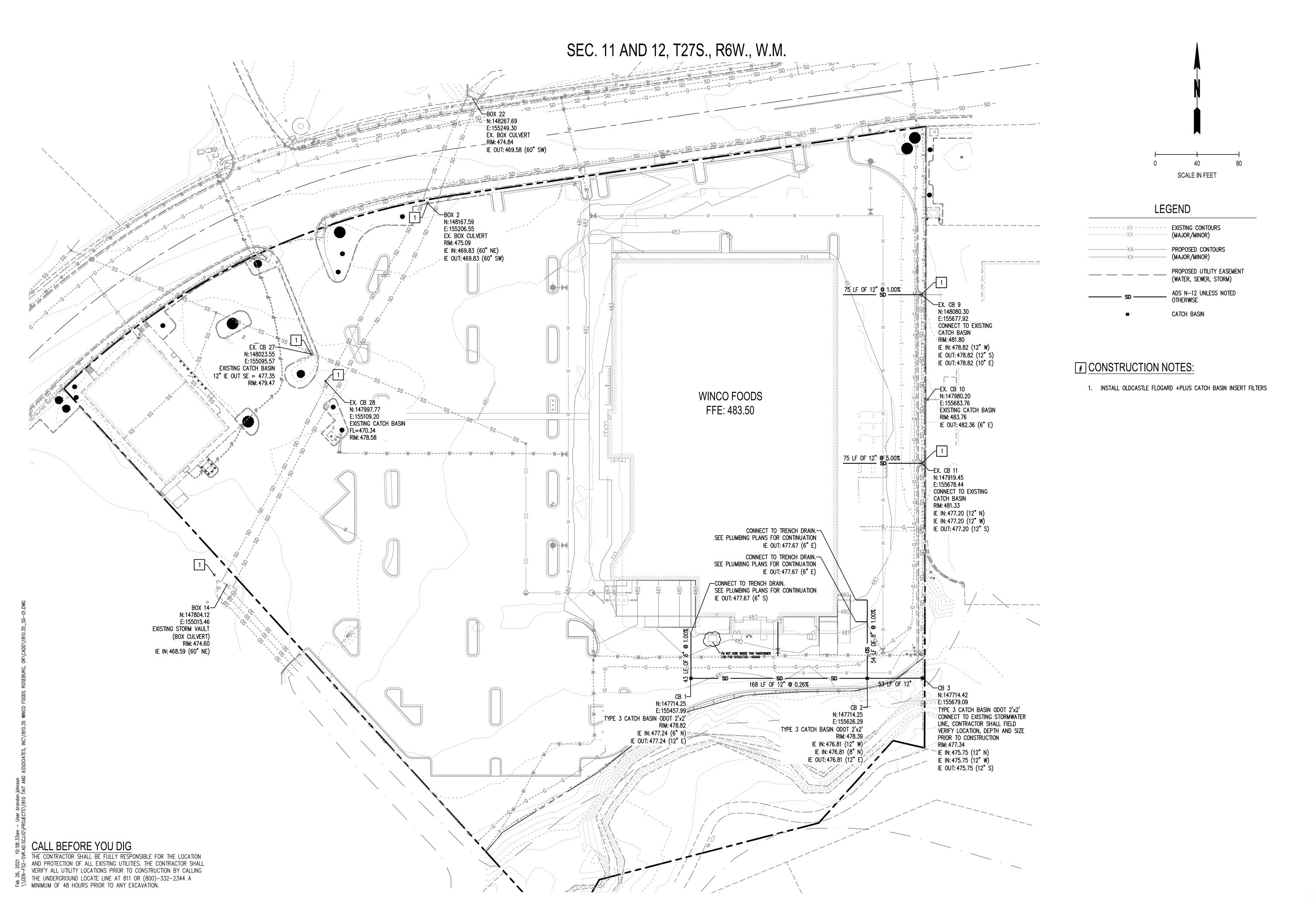
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MASTER UTILITYPLAN

MU-01



PRELIMINARY

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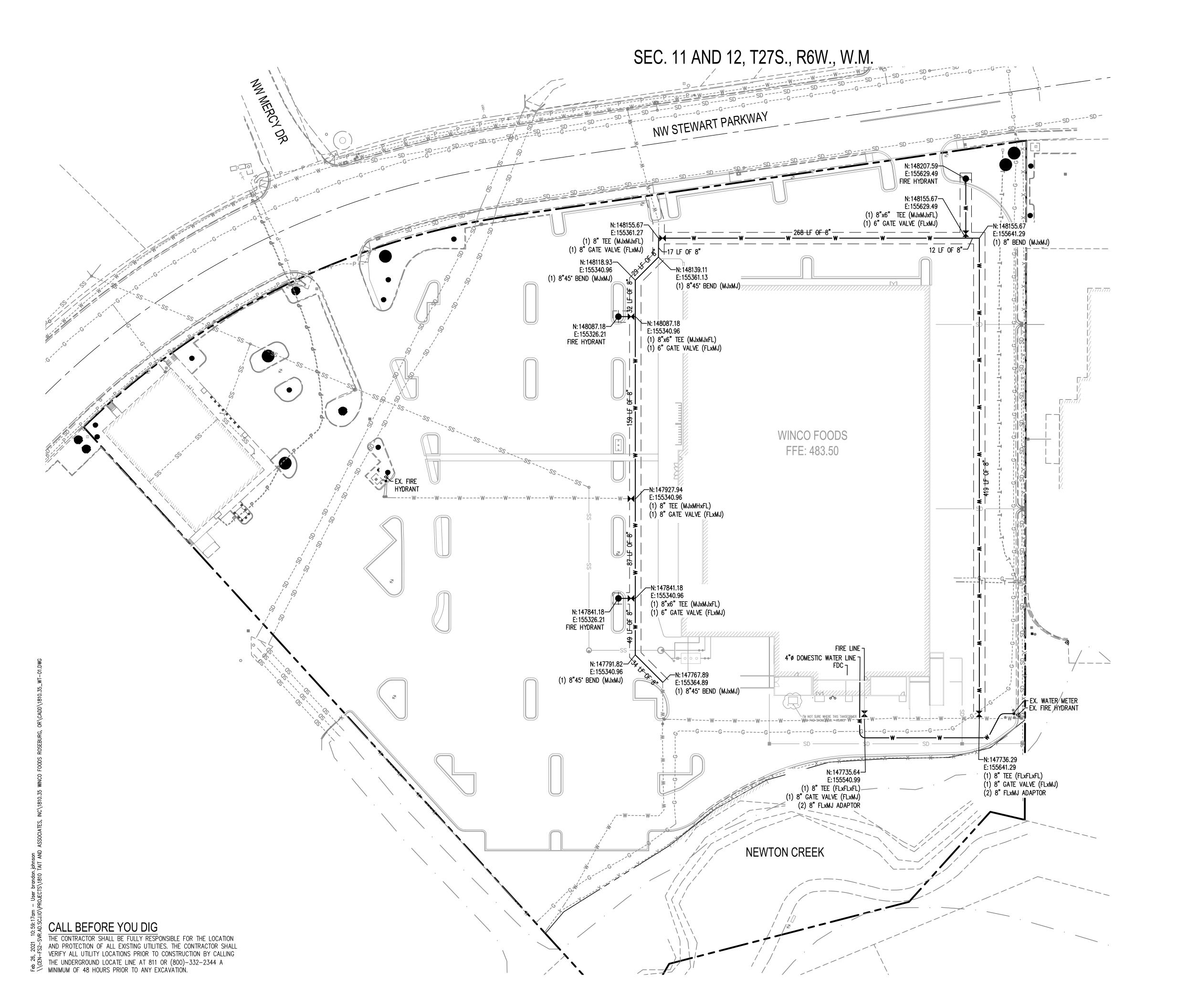
WINCO FOODS STORE NW STEWART PARKWA ROSEBURG, OREGON

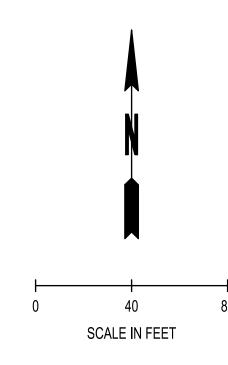
Minco Foods

STORMWATER

PLAN

SD-01





PROPOSED UTILITY EASEMENT (WATER, SEWER, STORM) ---- WATER LINE (ANSI/AWWA C900 SDR 18)

-W----W— EXISTING WATER LINE

HYDRANT ASSEMBLY

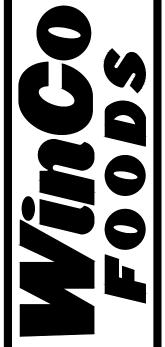
GENERAL WATER NOTES:

- MEGALUG RESTRAINED JOINTS (OR EQUAL) SHALL BE USED AT ALL WATER LINE FITTINGS, BENDS, TEES, ETC. THE CONTRACTOR SHALL COORDINATE WITH THE JOINT RESTRAINT MANUFACTURER AND SUPPLIER TO DETERMINE NUMBER OF JOINTS REQUIRED TO BE RESTRAINED AT EACH FITTING.
- 2. MAINTAIN 3-FT COVER OVER ALL WATER LINES (TYP)

PRELIMINARY

K. GANS J. BRUHN

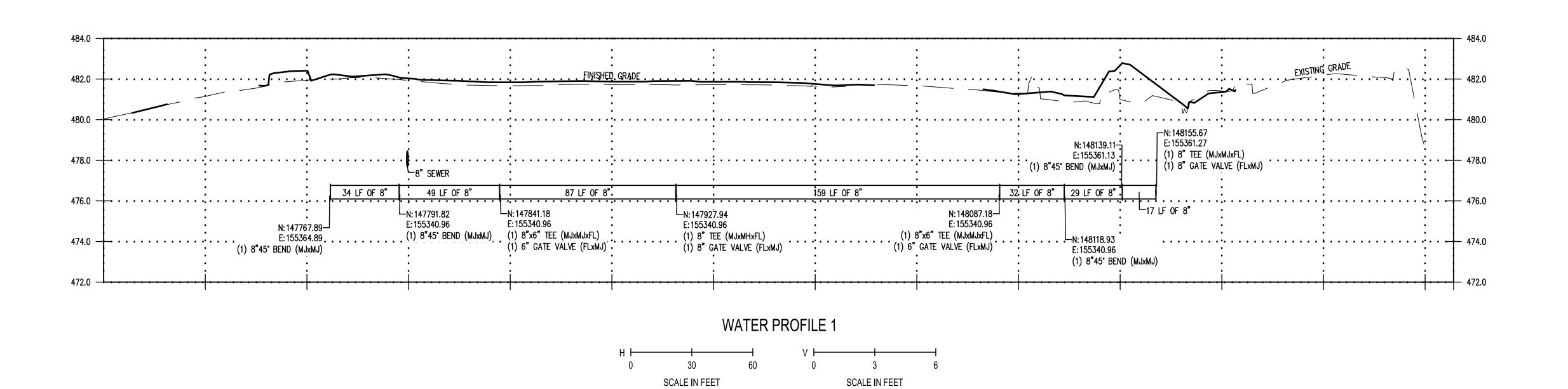


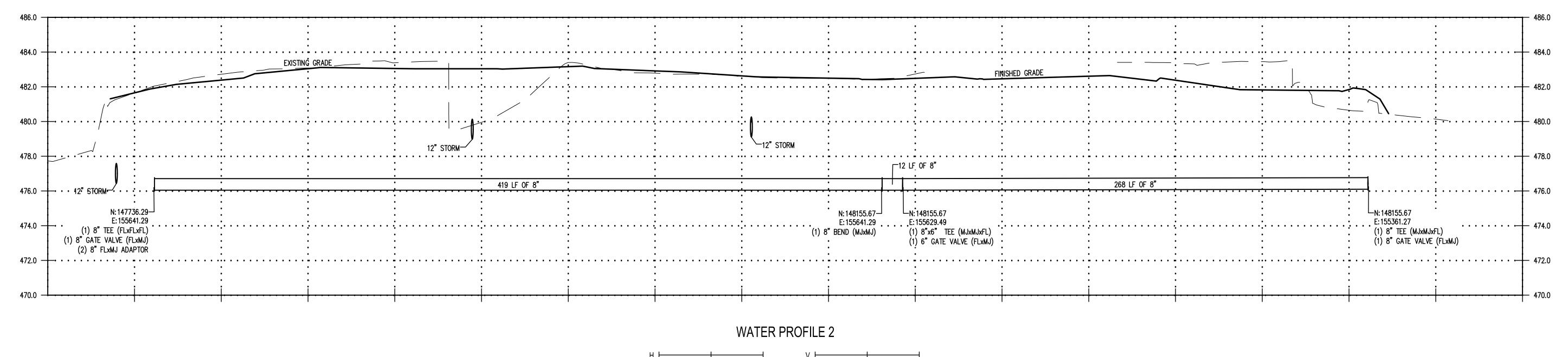


SHEET TITLE

WATER PLAN

WT-01



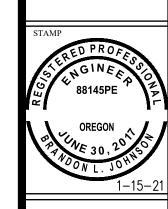


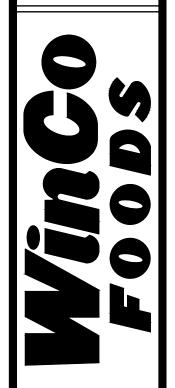
SCALE IN FEET

PRELIMINARY

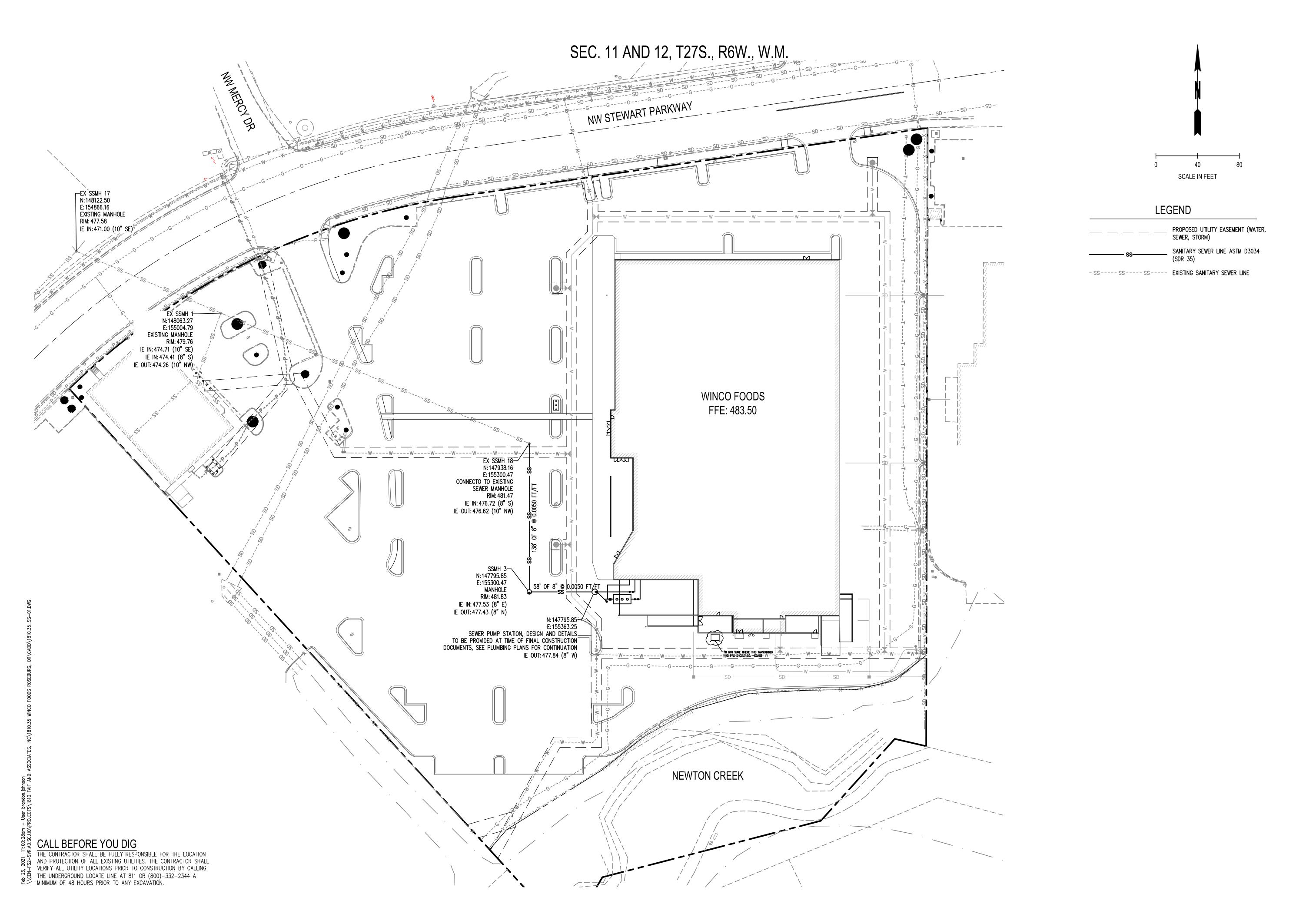
K. GANS

J. BRUHN





PROFILES



PRELIMINARY

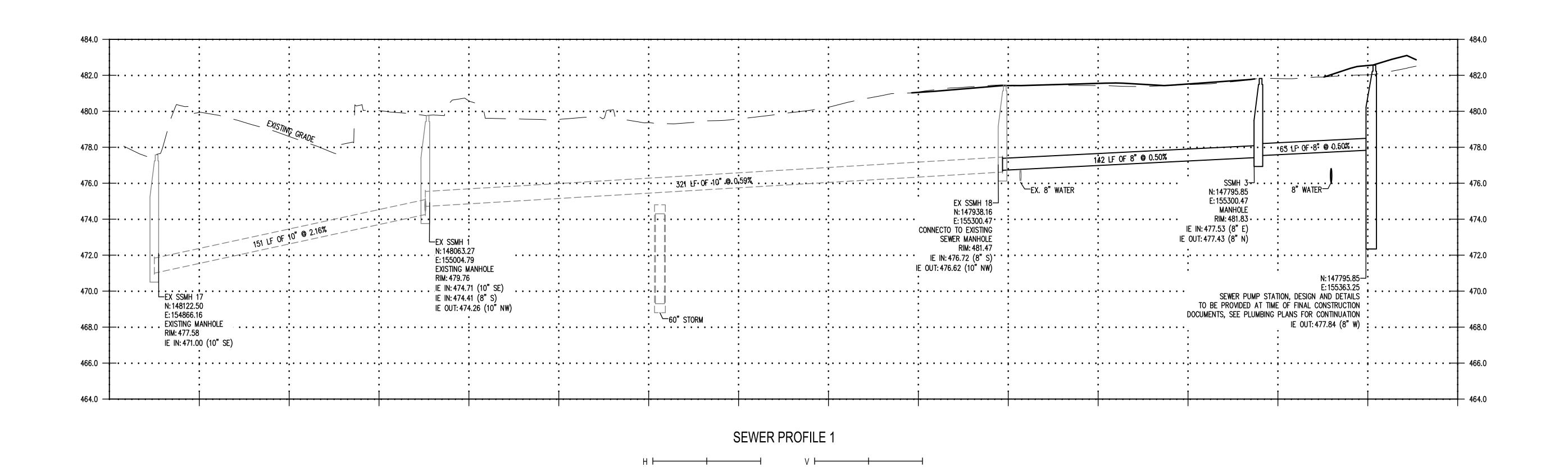
K. GANS J. BRUHN



SHEET TITLE

SEWER PLAN

SS-01



SCALE IN FEET

SCALE IN FEET

PRELIMINARY

K. GANS J. BRUHN

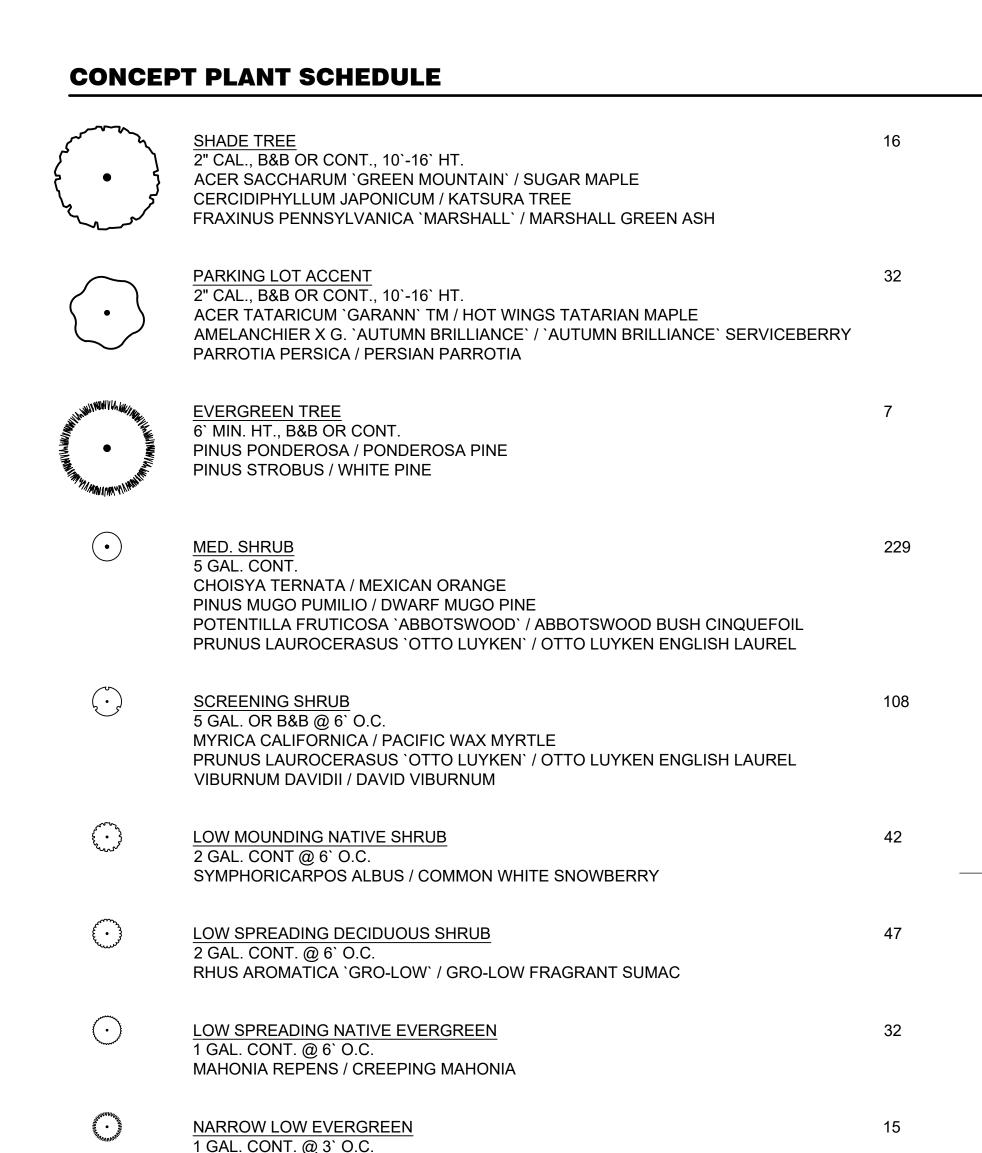


WINCO FOODS STO NW STEWART PARI ROSEBURG, OREGC



SHEET TITLE SEWER **PROFILES**

SS-02



6,528 SF GROUNDCOVER 1 GAL. CONT. 3` O.C. EUONYMUS COLORATUS / PURPLE WINTERCREEPER HELICTOTRICHON SEMPERVIRENS / BLUE OAT GRASS 189 HEMEROCALLIS X 'BLACK-EYED STELLA' TM / BLACK-EYED STELLA DAYLILY 189 189 PENNISETUM ALOPECUROIDES 'HAMELN' / HAMELN FOUNTAIN GRASS

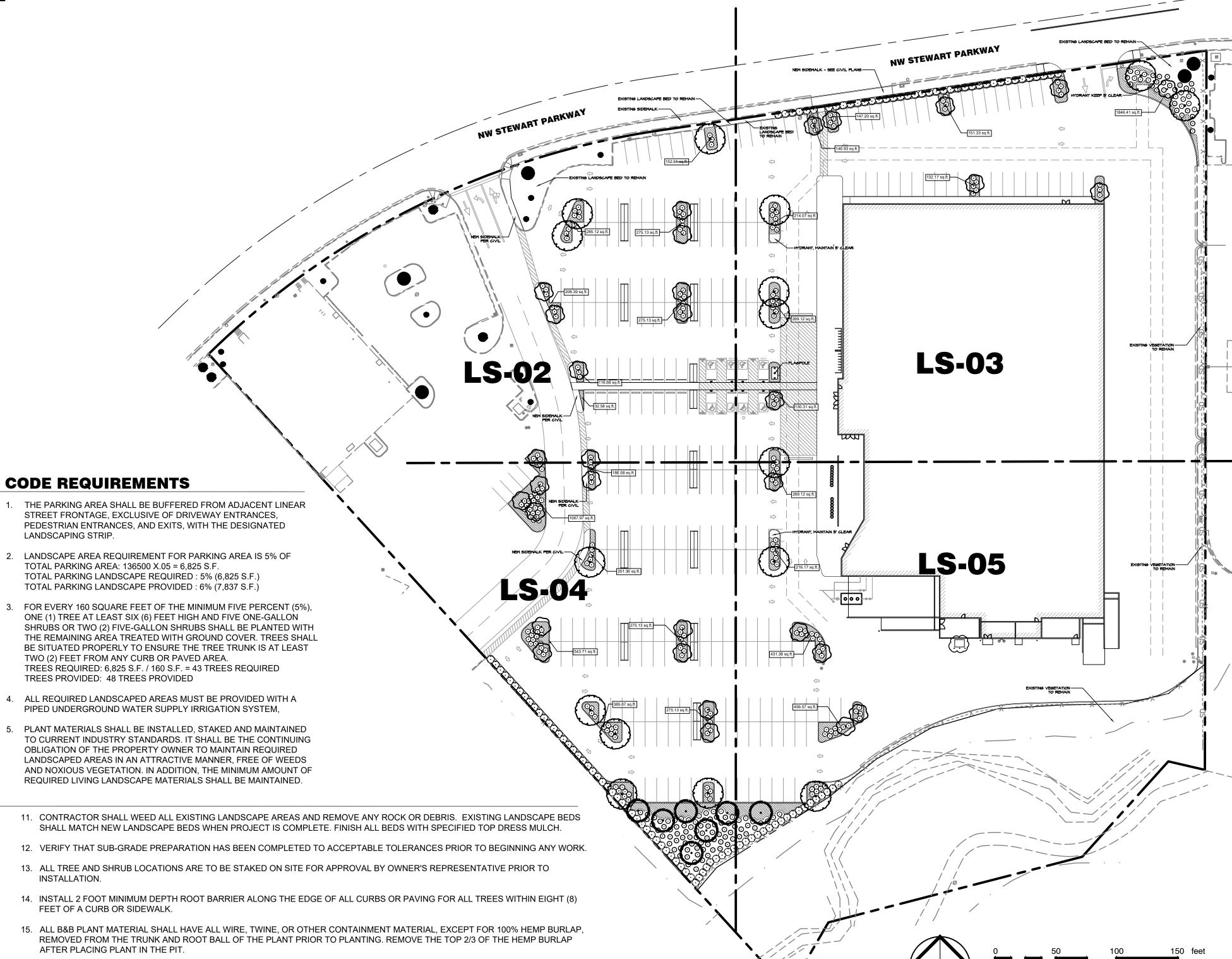
NANDINA DOMESTICA 'HARBOUR DWARF' / DWARF HEAVENLY BAMBOO

TREE PROTECTION & LANDSCAPE NOTES (ALL SHEETS)

- RETURN ALL AREAS TO THEIR PRE-CONSTRUCTION STATE AFTER LANDSCAPE INSTALLATION IS COMPLETE, AND ANY AND ALL PROTECTIVE MEASURES/ BARRIERS HAVE BEEN REMOVED.
- 2. THE LANDSCAPE ARCHITECTURAL SITE PLAN IS TO BE USED IN CONJUNCTION WITH THE CIVIL, IRRIGATION, MECHANICAL, ELECTRICAL, AND ARCHITECTURAL SITE PLANS TO FORM COMPLETE INFORMATION REGARDING THE SITE.
- CONTRACTOR SHALL PRESERVE AND PROTECT ALL EXISTING PLANTS TO REMAIN. PLANTING AREAS ARE SHOWN FOR CONTRACTOR REFERENCE REVIEW EXISTING PLANTS ON SITE AND ADJUST NEW PLANTINGS AS REQUIRED. PIT PLANT NEW PLANTS IN EXISTING LANDSCAPE AREAS. NO SOIL PREP IS REQUIRED IN EXISTING PLANTING AREAS.
- NEW LANDSCAPE AREAS SHALL BE IRRIGATED AS NECESSARY FOR ESTABLISHMENT. CONTRACTOR SHALL ENSURE EXISTING IRRIGATION IS FUNCTIONAL AND ALL NEW AREAS ARE ADDED TO THE SYSTEM.
- ALL NEW LANDSCAPE AREAS ARE TO BE EXCAVATED AND PREPARED PER NOTE #25. LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR ENSURING REQUIRED EXCAVATION IS PROVIDED BY OTHERS PRIOR TO EXCLUDING FROM THEIR CONTRACT.
- THE INFORMATION ON THIS SHEET IS INCOMPLETE UNLESS ACCOMPANIED BY THE CORRESPONDING SPECIFICATION SECTION(S) DEVELOPED FOR THIS PROJECT. REFER TO SPECIFICATIONS FOR ADDITIONAL INFORMATION.
- LANDSCAPE CONSTRUCTION SHALL CONFORM TO ALL APPLICABLE STATE AND LOCAL CODES AND SPECIFICATIONS.
- ALL MATERIAL AND WORKMANSHIP SHALL BE MAINTAINED AND GUARANTEED FOR A PERIOD OF 12 MONTHS FOLLOWING THE SUBSTANTIAL COMPLETION DATE.
- LANDSCAPE CONTRACTOR SHALL EXAMINE THE SITE CONDITIONS UNDER WHICH THE WORK IS TO BE PERFORMED, NOTIFY THE GENERAL CONTRACTOR IN WRITING OF UNSATISFACTORY CONDITIONS. DO NOT PROCEED UNTIL CONDITIONS HAVE BEEN CORRECTED.
- 10. BEFORE COMMENCING WORK, LANDSCAPE CONTRACTOR SHALL CONTACT APPROPRIATE UTILITY COMPANIES FOR UTILITY LOCATIONS, AND COORDINATE WITH GENERAL CONTRACTOR IN REGARD TO LOCATION OF PROPOSED UTILITIES, IRRIGATION SLEEVES, CONDUITS, ETC.

CALL BEFORE YOU DIG

THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO CONSTRUCTION BY CALLING THE UNDERGROUND LOCATE LINE AT 811 OR (800)-332-2344 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION.



16. DISTRIBUTE IMPORTED SANDY LOAM TOPSOIL (APPROVED BY THE LANDSCAPE ARCHITECT) IN PLANTERS AS INDICATED ON PLANS.

17. PLANTING AREA PREPARATION:

SPREADING OF PLANTING SOIL SHALL BE COMPLETED BY THE LANDSCAPE CONTRACTOR AND SUPERVISED BY THE GENERAL CONTRACTOR SO THAT GRADES ARE MET AS NOTED ON THE GRADING PLANS.

ALL NEW PLANTING AREAS SHALL BE EXCAVATED DOWN TO NATIVE SOIL OR 24 INCHES. SUB-GRADE SHALL BE SET TO WITHIN SIX (5) INCHES OF TOP OF ADJACENT CURBS TO ACHIEVE REQUIRED MULCH FINISH LEVEL OF 3 INCHES WITH 2 INCHES OF FREEBOARD ADJACENT TO PAVING.

WEED ALL EXISTING PLANTING BEDS AND PLACE PRE-EMERGENT PRIOR TO PLACING MULCH

- 18. ALL PLANTING AREAS SHOWN ON PLANS SHALL BE TOP DRESSED WITH A 3 INCHES OF BARK MULCH.
- 19. PLANT SYMBOLS SHALL DICTATE COUNT.
- 20. REFER TO PROJECT SPECIFICATIONS FOR MORE DETAILED INSTRUCTIONS.

PRELIMINARY T. GRANTHAM ← LA 634 🕏 OREGON CAPE ARCS EXP. 07/31/21

SHEET TITLE **OVERALL** LANDSCAPE

SHEET

LS-01

EXISTING BANDSCAPE BED TO REMAIN — NW STEWART PARKWAY EXISTING SIDEMALK 41 MED. SHRUB (3)-992 SF 152.61 sq.ft. SHADE TREE (I)--EXISTING LANDSCAPE BED TO REMAIN MED. SHRUB (8)— MED. SHRUB (8) PARKING LOT ACCENT (2)-285.12 sq.ft. 275.13 sq.ft. NEW SIDEWALK PER CIVIL -GROUNDCOVER (184 sf) GROUNDCOVER (175 sf)-MED. SHRUB (8)-208.39 sq.ft. PARKING LOT ACCENT (2) PARKING LOT ACCENT (2) 275.13 sq.ft. GROUNDCOVER (175 sf) - MED. SHRUB (3) PARKING LOT ACCENT (1) GROUNDCOVER (79 sf) ∐116.68 sq.ft.| GROUNDCOVER (33 sf) 32.58 sq.ft. NEW SIDEWALK — MED. SHRUB (3)--MED. SHRUB (1, -PARKING LOT ACCENT (1) PARKING LOT ACCENT (1) MATCH-LINE SEE SHEET LS-04

JJO\PROJECTS\1810 TAIT AND ASSOCIA

CALL BEFORE YOU DIG

THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO CONSTRUCTION BY CALLING THE UNDERGROUND LOCATE LINE AT 811 OR (800)–332–2344 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION.

REVISIONS PRELIMINARY

SCJ PROJECT NO.
#####.##
DRAWN
T. GRANTHAM

CHECKED

SUBMITTAL DATES

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;, CENTRALIA, WA 98531
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CONSULTING SE

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STAMP

LA 634

LA 634

OREGON

07/19/07

CAPE AR

EXP. 07/31/21

ICO FOODS STORE STEWART PARKWAY EBURG, OREGON

Foods Foods

SHEET TITLE

NORTHWEST

LANDSCAPE

PLAN

SHEET

LS-02

SCALE: 1" = 20'

SCREENING SHRUB
5 GAL. OR B&B @ 6` O.C.
MYRICA CALIFORNICA / PACIFIC WAX MYRTLE
PRUNUS LAUROCERASUS `OTTO LUYKEN` / OTTO LUYKEN ENGLISH LAUREL
VIBURNUM DAVIDII / DAVID VIBURNUM

POTENTILLA FRUTICOSA 'ABBOTSWOOD' / ABBOTSWOOD BUSH CINQUEFOIL

PRUNUS LAUROCERASUS 'OTTO LUYKEN' / OTTO LUYKEN ENGLISH LAUREL

LOW MOUNDING NATIVE SHRUB
2 GAL. CONT @ 6` O.C.
SYMPHORICARPOS ALBUS / COMMON WHITE SNOWBERRY

LOW SPREADING DECIDUOUS SHRUB

RHUS AROMATICA 'GRO-LOW' / GRO-LOW FRAGRANT SUMAC

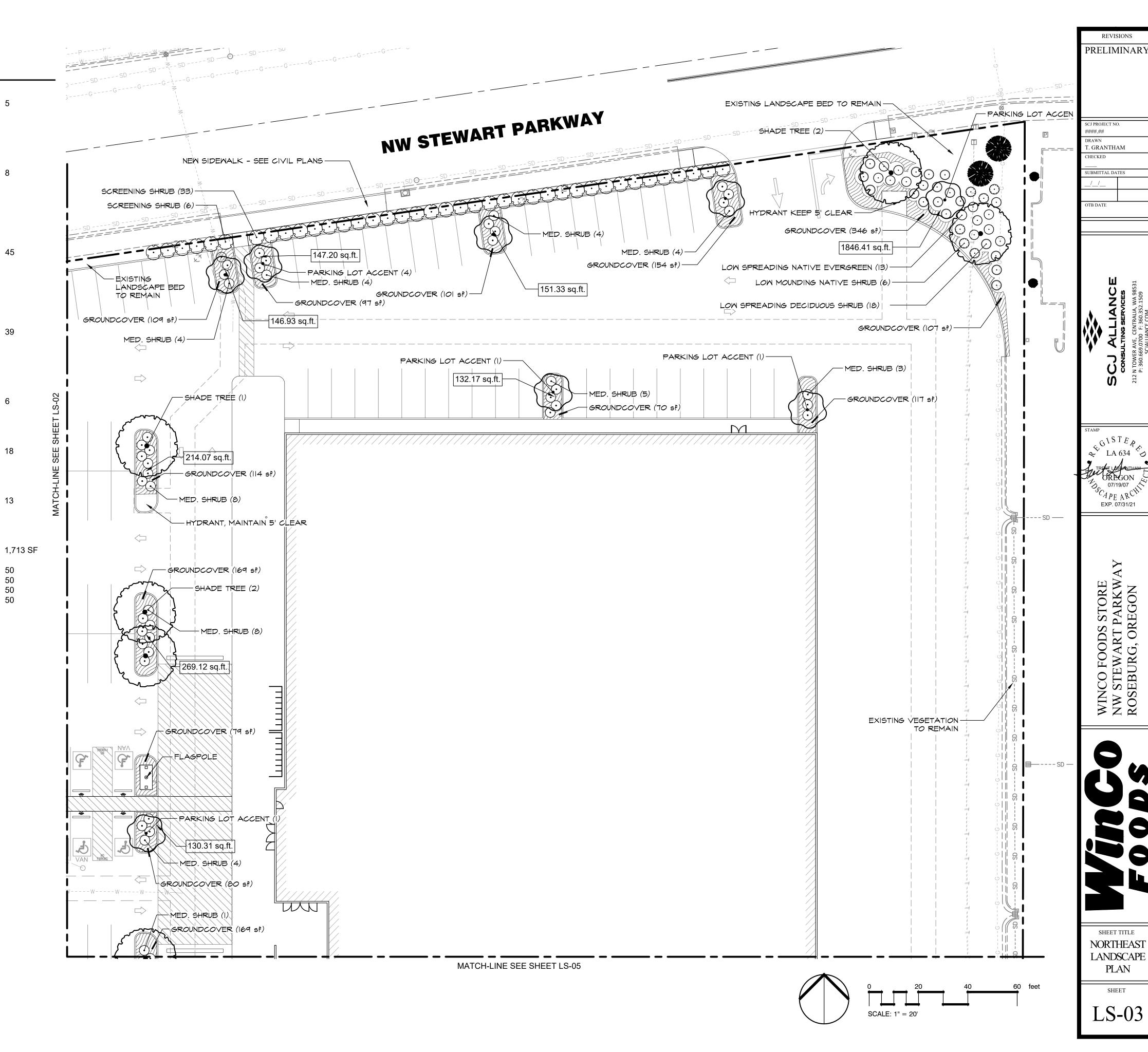
LOW SPREADING NATIVE EVERGREEN
1 GAL. CONT. @ 6` O.C.
MAHONIA REPENS / CREEPING MAHONIA

2 GAL. CONT. @ 6` O.C.

CHOISYA TERNATA / MEXICAN ORANGE

PINUS MUGO PUMILIO / DWARF MUGO PINE

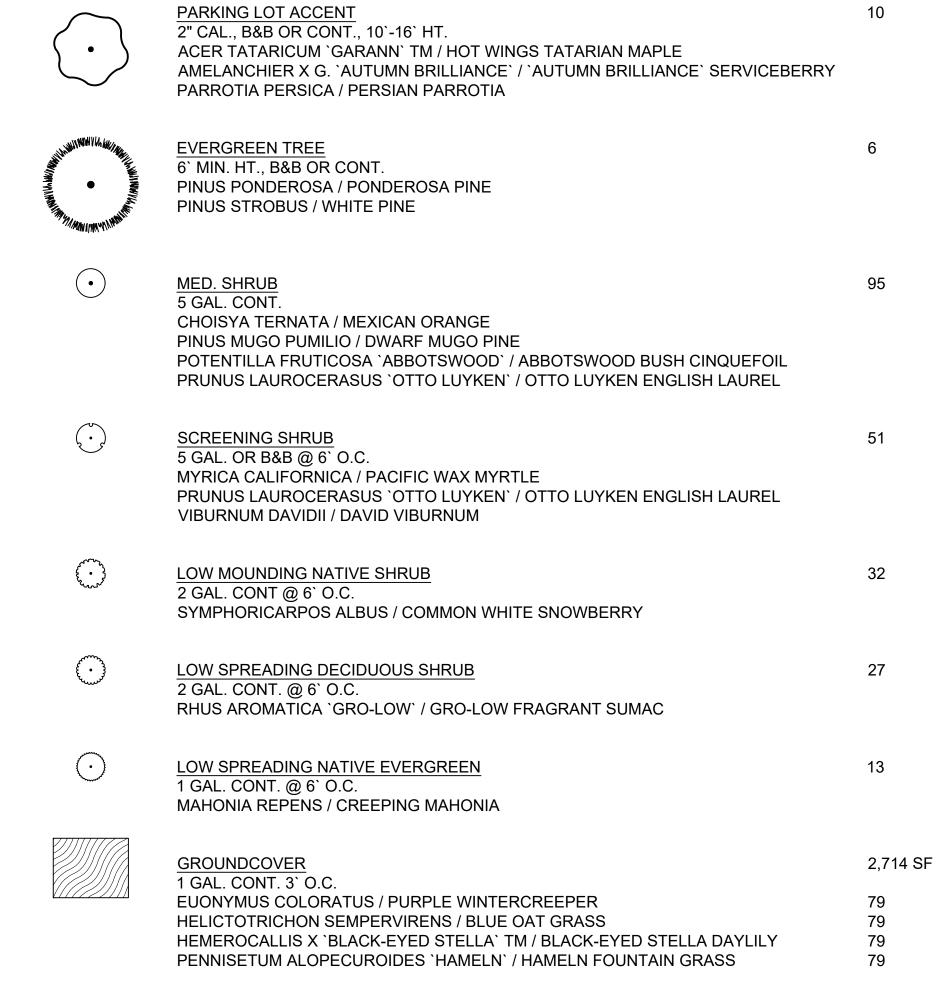
GROUNDCOVER
1 GAL. CONT. 3` O.C.
EUONYMUS COLORATUS / PURPLE WINTERCREEPER
HELICTOTRICHON SEMPERVIRENS / BLUE OAT GRASS
HEMEROCALLIS X `BLACK-EYED STELLA` TM / BLACK-EYED STELLA DAYLILY
PENNISETUM ALOPECUROIDES `HAMELN` / HAMELN FOUNTAIN GRASS
50



PP (2010) PP

CALL BEFORE YOU DIG

THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO CONSTRUCTION BY CALLING THE UNDERGROUND LOCATE LINE AT 811 OR (800)—332—2344 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION.



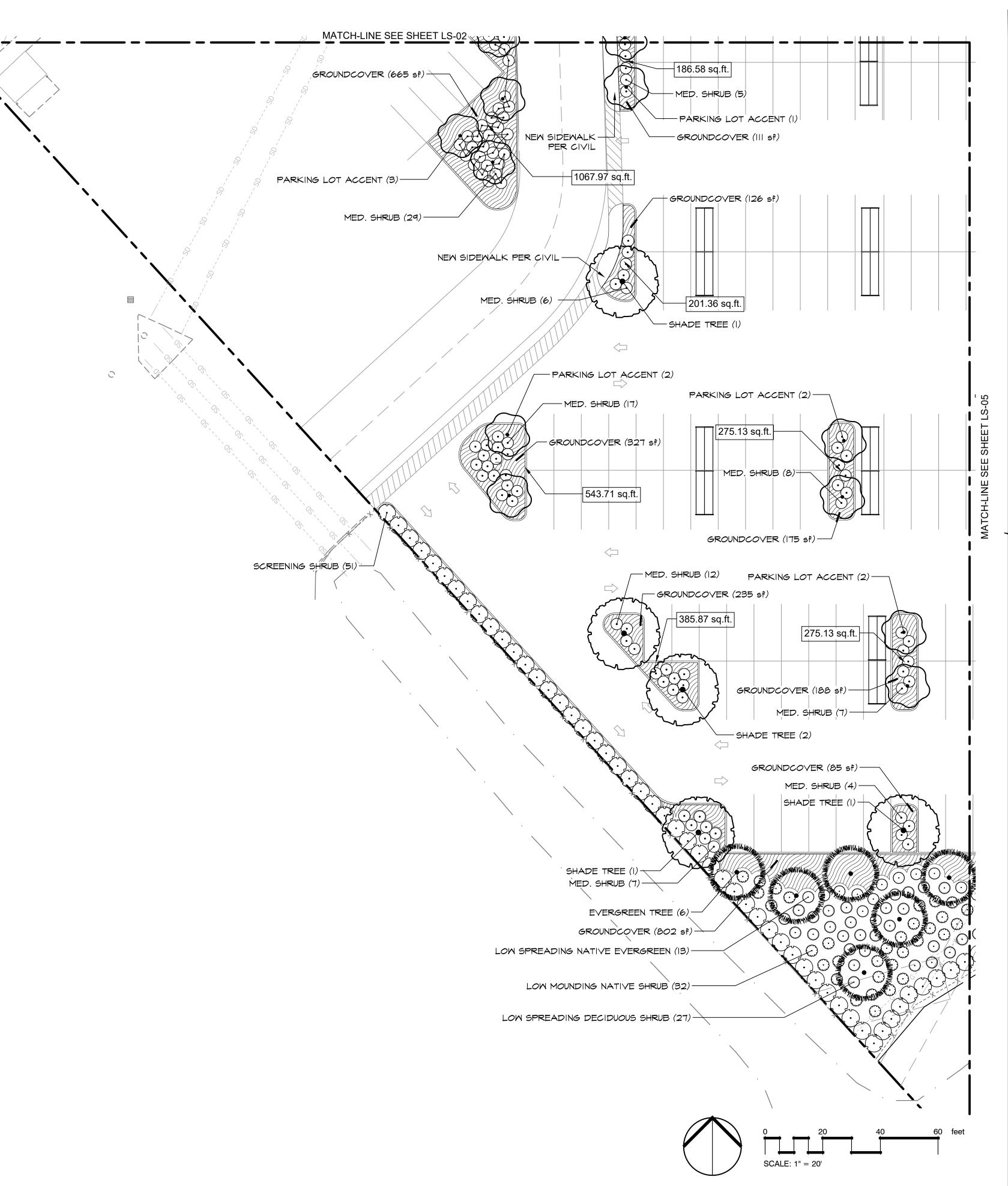
CONCEPT PLANT SCHEDULE LS-04

2" CAL., B&B OR CONT., 10`-16` HT.

ACER SACCHARUM 'GREEN MOUNTAIN' / SUGAR MAPLE

FRAXINUS PENNSYLVANICA 'MARSHALL' / MARSHALL GREEN ASH

CERCIDIPHYLLUM JAPONICUM / KATSURA TREE



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ALLIANCES

IN AVE, CENTRALIA, WA 98531

69.0700 F: 360.352.1509

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EXP. 07/31/21

WINCO FOODS STORE NW STEWART PARKWA' ROSEBURG, OREGON

Minco Foods

SHEET TITLE
SOUTHWEST
LANDSCAPE
PLAN

LS-04

SHADE TREE 2" CAL., B&B OR CONT., 10`-16` HT. ACER SACCHARUM 'GREEN MOUNTAIN' / SUGAR MAPLE

CERCIDIPHYLLUM JAPONICUM / KATSURA TREE FRAXINUS PENNSYLVANICA 'MARSHALL' / MARSHALL GREEN ASH

PARKING LOT ACCENT 2" CAL., B&B OR CONT., 10`-16` HT. ACER TATARICUM 'GARANN' TM / HOT WINGS TATARIAN MAPLE AMELANCHIER X G. `AUTUMN BRILLIANCE` / `AUTUMN BRILLIANCE` SERVICEBERRY PARROTIA PERSICA / PERSIAN PARROTIA

48

18

EVERGREEN TREE 6' MIN. HT., B&B OR CONT. PINUS PONDEROSA / PONDEROSA PINE PINUS STROBUS / WHITE PINE

MED. SHRUB 5 GAL. CONT. CHOISYA TERNATA / MEXICAN ORANGE PINUS MUGO PUMILIO / DWARF MUGO PINE POTENTILLA FRUTICOSA `ABBOTSWOOD` / ABBOTSWOOD BUSH CINQUEFOIL PRUNUS LAUROCERASUS 'OTTO LUYKEN' / OTTO LUYKEN ENGLISH LAUREL

SCREENING SHRUB 5 GAL. OR B&B @ 6` O.C. MYRICA CALIFORNICA / PACIFIC WAX MYRTLE PRUNUS LAUROCERASUS 'OTTO LUYKEN' / OTTO LUYKEN ENGLISH LAUREL VIBURNUM DAVIDII / DAVID VIBURNUM

LOW MOUNDING NATIVE SHRUB 2 GAL. CONT @ 6` O.C. SYMPHORICARPOS ALBUS / COMMON WHITE SNOWBERRY

LOW SPREADING DECIDUOUS SHRUB 2 GAL. CONT. @ 6` O.C. RHUS AROMATICA `GRO-LOW` / GRO-LOW FRAGRANT SUMAC LOW SPREADING NATIVE EVERGREEN

1 GAL. CONT. @ 6` O.C. MAHONIA REPENS / CREEPING MAHONIA NARROW LOW EVERGREEN

1 GAL. CONT. @ 3` O.C. NANDINA DOMESTICA 'HARBOUR DWARF' / DWARF HEAVENLY BAMBOO

GROUNDCOVER 1,389 SF 1 GAL. CONT. 3' O.C. EUONYMUS COLORATUS / PURPLE WINTERCREEPER HELICTOTRICHON SEMPERVIRENS / BLUE OAT GRASS 41 HEMEROCALLIS X `BLACK-EYED STELLA` TM / BLACK-EYED STELLA DAYLILY 41 PENNISETUM ALOPECUROIDES 'HAMELN' / HAMELN FOUNTAIN GRASS 41

MATCH-LINE SEE SHEET LS-03 - NARROW LOW EVERGREEN (8) SHADE TREE (269.12 sq.ft. - NARROW LOW EVERGREEN (7) — PARKING LOT ACCENT (1) 🕻 - HYDRANT, M'AINTAIN 5' CLEAR EXISTING VEGETATION-216.17 sq.ft. TO REMAIN \mathbb{W} -SHADE TREE (1) - GROUNDCOVER (116 sf) GROUNDCOVER (268 sf)-MED. SHRUB (13)--PARKING LOT ACCENT (2) 431.38 sq.ft. EXISTING VEGETATION - TO REMAIN GROUNDCOVER (281 sf) GROUNDCOVER (59 sf) MED. SHRUB (6)--GROUNDCOVER (495 sf) LOW SPREADING NATIVE EVERGREEN (6) EVERGREEN TREE (I -LOW MOUNDING NATIVE SHRUB (4) -SCREENING SHRUB (18) LOW SPREADING DECIDUOUS SHRUB (2)

CALL BEFORE YOU DIG THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO CONSTRUCTION BY CALLING THE UNDERGROUND LOCATE LINE AT 811 OR (800)-332-2344 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION.

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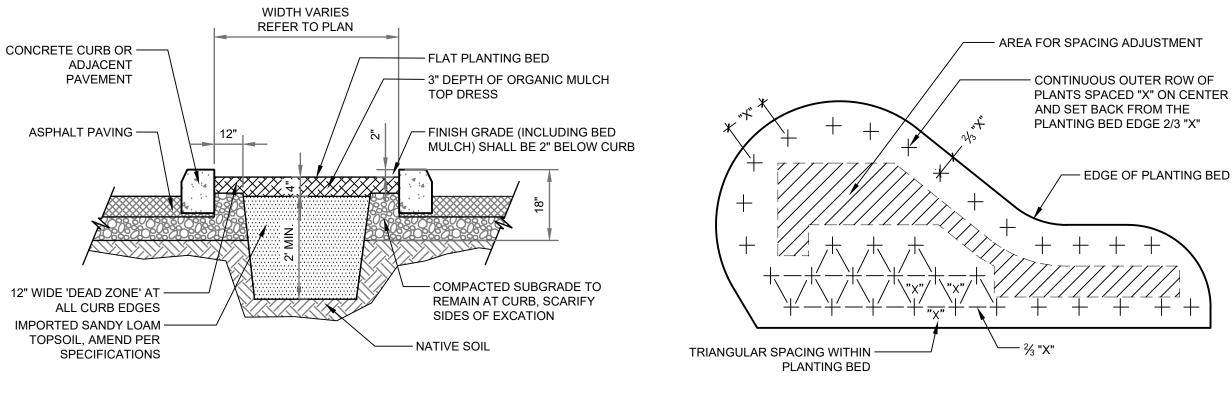
SOUTHEAST LANDSCAPE PLAN

SHEET

LS-05

SHRUB FLARE SHALL BE CLEARLY VISIBLE AFTER PLANTING -18" DIAMETER WATERING PLANTING PIT ROOTBALL SHALL BE A FERTILIZER TABLETS, DEPTH MIN. OF TWICE 2"-6" DEPTH THE ROOTBALL 6" MIN. DEPTH REMOVE BURLAP FROM A MIN. OF $\frac{2}{3}$ FROM ROOTBALL AFTER PLACING SHRUB IN PLANTING ROOTBALL HOLE (REMOVE COMPLETELY IF DIAMETER FIBERGLASS OR TREATED). - BACKFILL SOIL MIX A MINIMUM OF TWICE THE ROOT- BALL WIDTH

SHRUB PLANTING DETAIL (B&B OR CONT.)



P-WI-ROS-17

GENERAL CONTRACTOR SHALL REMOVE EXCESS ASPHALT PAVEMENT AND SUB-BASE MATERIAL DOWN TO NATIVE SOIL.

PARKING ISLAND PLANTER

P-WI-ROS-04

SPECIFIED PLANT SPACING = "X" INDIVIDUAL PLANTS REPRESENTED AS: +

PLANT SPACING DETAIL

P-WI-ROS-01

TREE PLANTING & STAKING DETAIL

2. LANDSCAPE CONTRACTOR SHALL INCLUDE IN THEIR BID A REPORT BY A CERTIFIED ARBORIST WITH PRUNING

3. IMPORTANT: ALL TREES INDICATED AS "TO REMAIN" MUST BE RETAINED. FAILURE TO RETAIN ANY INDICATED TREE WILL LANDSCAPE ARCHITECT, FORESTER AND/OR CITY (AS APPLICABLE) IF IT IS DETERMINED THAT A TREE CANNOT BE

4. THE CONTRACTOR SHALL UTILIZE A CERTIFIED ARBORIST TO TIE BACK ALL FLEXIBLE LIMBS AND OVERHEAD BRANCHES WHICH MAY BE DAMAGED BY THE PASSAGE OR ACTIVITY OF CONSTRUCTION EQUIPMENT.

5. PLACING MATERIALS NEAR TREES: NO PERSON MAY CONDUCT ACTIVITY WITHIN THE DRIP LINE OF ANY EXISTING TREE, INCLUDING BUT NOT LIMITED TO PARKING EQUIPMENT, PLACING SOLVENTS, STORING BUILDING MATERIAL AND SOIL

6. CONTRACTOR SHALL NOT CUT ANY ROOT OVER 2" IN DIAMETER.

7. ATTACHMENTS TO TREES: DURING CONSTRUCTION, NO PERSON SHALL ATTACH ANY OBJECT TO ANY EXISTING TREE.

8. PROTECTIVE BARRIER. BEFORE DEVELOPMENT, LAND CLEARING, FILLING OR ANY LAND ALTERATION, THE CONTRACTOR:

SHALL COVER WITH MULCH TO A DEPTH OF AT LEAST SIX (6) INCHES OR WITH PLYWOOD OR SIMILAR MATERIAL THE AREAS ADJOINING THE CRITICAL ROOT ZONE (DRIP LINE) OF A TREE IN ORDER TO PROTECT ROOTS FROM DAMAGE CAUSED BY HEAVY EQUIPMENT.

IN THE DRIP LINES OF EXISTING TREES. SHALL ENSURE THAT ANY LANDSCAPING DONE WITHIN THE DRIP LINE

AND ALL PROTECTIVE MEASURES/ BARRIERS HAVE BEEN REMOVED.

TREE PROTECTION NOTES (ALL SHEETS)

1. THE CONTRACTOR SHALL ARRANGE A MEETING WITH THE OWNER'S REPRESENTATIVE TO IDENTIFY EXISTING TREES THAT MAY BE IMPACTED BY CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE TO LANDSCAPE FEATURES LOCATED ON AND OFF THE CONSTRUCTION PROPERTY THAT RESULTS FROM THE FAILURE TO SCHEDULE AND ATTEND THE PRE-INSTALLATION MEETING.

P-WI-ROS-08

RECOMMENDATIONS AND A COST TO COMPLETE ALL PRUNING OF THE EXISTING TREES.

RESULT IN CONTRACTOR REPLACING TREE PER THE CITY OR OTHER REQUIREMENTS. CONTRACTOR SHALL NOTIFY THE

DEPOSITS, DUMPING CONCRETE WASH OUT.

b. SHALL PROHIBIT EXCAVATION OR COMPACTION OF EARTH OR OTHER POTENTIALLY DAMAGING ACTIVITIES WITH SHALL BE ACCOMPLISHED WITH LIGHT MACHINERY OR HAND LABOR.

9. RETURN ALL AREAS TO THEIR PRE-CONSTRUCTION STATE AFTER LANDSCAPE INSTALLATION IS COMPLETE, AND ANY

THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR THE LOCATION AND PROTECTION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO CONSTRUCTION BY CALLING THE UNDERGROUND LOCATE LINE AT 811 OR (800)-332-2344 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION.

LA 634

OREGON 07/19/07 SCAPE ARCH EXP. 07/31/21

REVISIONS

PRELIMINARY

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T. GRANTHAM

SHEET TITLE

DETAIL SHEET

LS-06



Stormwater Technical Memo

To City of Roseburg, Department of Public Works

Prepared by: Jason Bruhn

Approved by: Brandon Johnson, PE

Date: January 15, 2021

Project: WinCo Foods, LLC, Roseburg, Oregon

Subject Stormwater Review Technical Memorandum

1. Purpose

The purpose of this technical memorandum is to describe the stormwater improvements that are to be completed as part of the proposed WinCo Foods store located at 2757 Northwest Stewart Parkway in Roseburg, Oregon. The site is shown in Figure 1.



Newton Cree

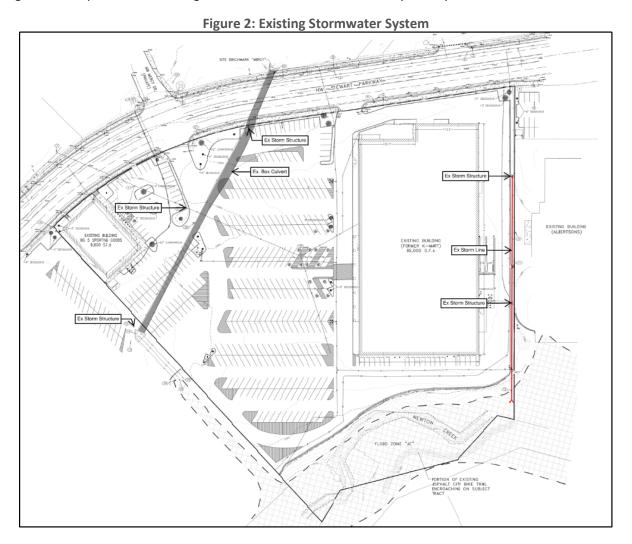


2. Existing Conditions

The site of the proposed WinCo Foods store is located on the site of a former K-Mart store. The parcel, Property Tax ID 270612B01502, (Douglas County, Oregon), is approximately 9.47 acres. The site is presently developed with an approximately 85,000-sf retail building, surrounding asphalt paved parking and drive aisles, landscape islands, and is served by water, sewer, gas, electricity, communication, and stormwater collection and conveyance utilities.

The existing stormwater collection and conveyance system consists of stormwater structures that connect to an existing box culvert that run under the existing parking. Also, there is a second storm line that runs along the east property line. Both the box culvert and the east storm line drain to Newton Creek.

See Figure 2 for a plan of the existing stormwater collection and conveyance system.





2.1 Soils

Terracon Consultants, Inc. performed a geotechnical investigation at the site and prepared a Geotechnical Engineering Report. In the report, the site geotechnical characterization is described:

- In general, the site subsurface conditions encountered by Terracon in their explorations consist of 1 to 7 feet of undocumented fill over native, medium stiff to stiff fat clay soils. The highly plastic, fat clay soils have varying amounts of gravel and sand fractions and are underlain by a probable siltstone/mudstone bedrock at a depth between 25 and 29 feet below the ground surface (where encountered in our explorations).
- Groundwater was observed by Terracon at depths ranging from 3.5 to 18 feet
- No infiltration testing was performed.

The Terracon Geotechnical Engineering Report is in Appendix C of this memo.

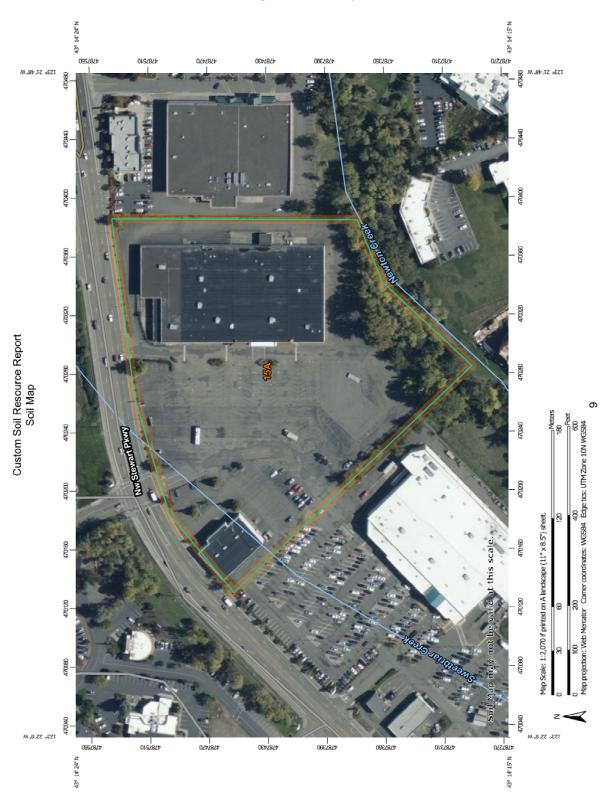
The soil identified by the NRCS on the project site is Bashaw clay (hydrologic soil group D), see Figure 3. A map of the site with the soil types is found in Figure 4. The complete NRCS Soil Resource Report is in Appendix BError! Reference source not found. of this memo.

Figure 3: Project Site Soils

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
15A	Bashaw clay, 0 to 1 percent slopes	9.6	100.0%
Totals for Area of Interest		9.6	100.0%



Figure 4: Soil Map





Flood Hazard Zone

The project site is located in a Zone X as determined by FEMA. Zone X is an area of moderate flood hazard, usually the area between the limits of the 100-year and 500-year floods. Please see the FIRMette in Figure 5.

National Flood Hazard Layer FIRMette 👺 FEMA Legend SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT Without Base Flood Elevation (BFE) With BFE or Depth Zone AE, AO, AH, VE, AR Regulatory Floodway Future Conditions 1% Annual Chance Flood Hazard Zone X Area with Reduced Flood Risk due to Levee, See Notes, Zone X AREA OF MINIMAL FLOOD HAZARD Area with Flood Risk due to Levee Zone D NO SCREEN Area of Minimal Flood Hazard Zone X Effective LOMRs OTHER AREAS GENERAL - - Channel, Culvert, or Storm Sewer STRUCTURES | | | Levee, Dike, or Floodwall 20.2 Cross Sections with 1% Annual Chance TANNUAL CHANCE FLOOD DISCHA Limit of Study TT27S RR6W S1' 275' of Roseburg Jurisdiction Boundary Coastal Transect Baseline Profile Baseline Hydrographic Feature 2/17/2010 Digital Data Available No Digital Data Availabl MAP PANELS FLOODWAY CONTRIBED IN CHANNEL The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 1/15/2021 at 11:09 AM and does not was exported on 11 10/2021 at 11.09 Am and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time. This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for 1:6,000

Figure 5: National Flood Hazard FIRM

Critical Areas 2.3

Newton Creek is located at the south end of the property an is in a flood zone. This area will remain undisturbed as part of this project.

, semap: USGS National Map: Orthoimagery: Data refreshed October, 2020

regulatory purposes.



3. Proposed Conditions

The project will consist of removing the existing K-Mart building and constructing a new WinCo Foods building in the same location. Also included is the construction of landscape islands in the parking lot and removing a portion of the asphalt at the south end of the parking lot. The ADA parking stall will be regraded to provide a ADA compliant pathway to the building.

The overall impervious surface for the project will be reduced from the installation of landscape islands and the removal of the asphalt at the south end of the parking lot. These areas will be planted trees, and shrubs. Please see the landscape plans for the project.

A water line will be constructed around the building and the sewer service reconnected to the building. Asphalt around the building will be replaced along with what is required for the installation of utilities and other grading required for the installation of the truck and trash compactors.



SEC. 11 AND 12. T27S., R6W., W.M.

WINGSTEMART PARSONNA

WINCO FOODS

FIFE-4-83-39

Description

MENTION CREEK

Figure 6: Site Plan

4. Stormwater System

4.1 Conveyance

Stormwater from the project site will continue to sheet flow to existing stormwater structues. From the existing stormwater structues the stormwater flows to an existing box culvert located under the parking lot or to an existing storm line along the east property line and then to Newton Creeek. This flow path and existing stormwater conveyance system will not change as part of this project.



4.2 Treatment

Stormwater treatment will be provided by installing Flogard +Plus Catch Basin Insert Filters in all the stormwater structures in the parking lot.

Per Oldcastle Infrastructure Flogard +Plus Technical Note the Flogard +Plus Catch Basin Insert Filters filter have 80% TSS removal and 70-80% oil & gas removal. Please see the Technical Note in Appendix "A".

END OF STORMWATER TECHNICAL MEMO

Prepared by Jason Bruhn

Anniewed hy Prandon Johnson PE

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Land Use Regulation Text Amendments File No. LUDR-21-001

Meeting Date: April 5, 2021 Completeness Date: N/A 120-Day Limit: N/A

Staff Contact: John K. Lazur, Associate Planner

Applicant: City of Roseburg

Request: Amend the Roseburg Municipal Code to reflect changes to the City of Roseburg's

mobility standards as recommended by the 2019 Transportation System Plan Update.

ISSUE STATEMENT AND SUMMARY

Staff is proposing to amend the current mobility standards found in RMC 12.06.020(C) which address when a Traffic Impact Study (TIS) is required for a proposed development.

BACKGROUND/ANALYSIS

Section 12.06.020(C) of the Roseburg Municipal Code requires developers to submit a TIS to accompany a proposal for development such as a new commercial building or housing development. A TIS is a document prepared by a traffic engineer who studies the operational characteristics of any intersection that might be negatively affected by future traffic impacts as a result of development. The goal of the TIS is to evaluate whether or not a particular intersection is operating below the mobility standard set by the City.

The traffic mobility standard refers to a value or measurement, which represents the movement of traffic at an intersection. In a TIS, the intersection mobility standard must meet the threshold or targets set by the City to help measure the efficiency of an intersection. If a TIS determines that a particular development will cause an intersection to exceed the mobility standard, it must recommend improvements like traffic signal upgrades or roadway upgrades like turn lanes, medians, or other traffic calming measures to keep the intersection at or below the mobility standard. Therefore, the mobility standard helps the City maintain levels of acceptable congestion on a given street.

There are two mobility standards the City utilizes. Level of Service (LOS) and volume/capacity ratio (v/c). LOS is a letter value that is assigned to measure the performance of an intersection. For example, LOS A, B, and C indicate conditions where traffic moves without significant delays over the peak hour travel demand (ie. "rush hour"). Traffic gets progressively worse through LOS D and E and finally, LOS F represents conditions where delay has become excessive and demand has exceeded capacity. An example of LOS F would be a signal needing 2 or more cycles to clear an intersection.

V/c ratio is a numerical measurement of how close to capacity a roadway is operating. For example, a v/c ratio of 0.75 means the roadway is using 75% of its capacity. This means that there is enough space for a 25% increase in volume before there are negative traffic impacts such as traffic delays, bottlenecks or "traffic jams".

Current regulations call for different mobility standards to apply based on roadway classification. Based on the recommendations from the TSP update, the City is requesting to allow an increase of 5% to the capacity threshold for Collector streets and a 10% increase to the capacity threshold for Arterial streets. Equally, LOS will allow for a few more seconds of delay at signalized intersections before requiring efficiency upgrades.

The 2019 TSP update recommended that the City of Roseburg mobility targets proposed be consistent across the city and a dual standard based on v/c ratios and LOS is proposed. City streets shall maintain a LOS of "E" and v/c no worse than 0.95 during the peak hour of the day. Roadways that are under ODOT or Douglas County jurisdiction, the mobility targets of those agencies apply, unless no other mobility target has been adopted.

It is anticipated that over time, intersections such as Garden Valley Boulevard at Stewart Parkway or Stephens Street will become increasingly more congested. With new development and increased employment opportunities, our streets must accommodate more vehicles. However, adding more capacity (more lanes, raised medians, eliminating existing access points, etc.) may not be cost effective or practical. These code amendments ultimately help to balance reasonable traffic congestion with significant capacity improvements. Adjusting this balancing point will help the City to more effectively promote future development, while insuring reasonable traffic expectations for our citizens as they navigate City streets.

The attached Findings of Fact and Order provides justification for the text amendments.

OPTIONS:

- 1. Adopt proposed Findings of Fact recommending the City Council approve the text amendments.
- 2. Adopt proposed Findings of Fact recommending the City Council deny the text amendments.
- 3. Adopt modified Findings of Fact.

RECOMMENDATION:

Given the proposal meets applicable criteria, staff recommends the Planning Commission recommend City Council approve the Findings of Fact as presented.

SUGGESTED MOTION:

I MOVE TO ADOPT THE FINDINGS OF FACT AS PRESENTED, AND RECOMMEND CITY COUNCIL **APPROVE** THE PROPOSED TEXT AMENDMENTS FILE NO. LUDR-21-001.

EXHIBITS:

Exhibit A - Findings of Fact and Order Exhibit B - Text Amendments

In the Matter of the Legislative Action by the City of Roseburg

) Text Amendment) File # LUDR-21-001

BEFORE THE ROSEBURG PLANNING COMMISSION FINDINGS OF FACT AND ORDER

I. NATURE OF AMENDMENTS

The Community Development Department proposes changes to the Land Use and Development Regulations amending the Roseburg Municipal Code (RMC), addressing the mobility standard for a Traffic Impact Study (TIS) when required for a proposed development.

II. PUBLIC HEARING

A public hearing was held on the proposed amendments before the Roseburg Planning Commission on April 5, 2020. At the hearing, the Planning Commission reviewed Land Use File LUDR-21-001 for legislative text amendments and it was made part of the record.

III. FINDINGS OF FACT

A. <u>EXISTING CONDITIONS</u>

- The Planning Commission takes official notice of the Roseburg Urban Area Comprehensive Plan adopted by City Council Ordinance #2980 on December 9, 1996 and of the Roseburg Land Use and Development Ordinance #2363, as originally adopted July 1, 1982, as both may have been amended from time-totime.
- Notice of the public hearing was given by publication in the News-Review, a
 newspaper of general circulation, at least 10 days prior to the hearing.
 Opportunities were provided for all interested parties to be involved in the
 planning process through the public hearing.
- 3. The proposal is to legislatively amend text within the Land Use and Development Regulations in the Roseburg Municipal Code.

B. PROPOSAL

The full text of the changes made in this amendment are attached.

C. AGENCY COMMENTS

Coordination occurred between the Community Development Department, Public Works Departments, and ODOT while the proposed amendments were being drafted. The Oregon Department of Land Conservation and Development provided the following comments:

"...We found that the proposal to adjust the V/C ratio is definitely a move in the right direction. However, we suggest that the V/C could be increased to 1.0, which is the point at which vehicle capacity matches traffic and the roadway is being used at peak efficiency. Alternatively, and an even better approach, would be to get rid of LOS and V/C entirely and use a VMT metric instead...In any case, the current proposal is an improvement..."

D. PUBLIC COMMENTS

No public comments were received prior to the hearing.

F. REVIEW CRITERIA

Text amendments to the Land Use and Development Regulations identified in RMC Title 12 require legislative action. Procedures in which a legislative action must occur are contained in RMC Section 12.10.020. Section 12.10.020(F)(2) requires that all legislative action proposals be analyzed for consistency with the policies of the Comprehensive Plan, Statewide Planning Goals, and other provisions of the Code.

The Roseburg Comprehensive Plan, as mandated by ORS Chapter 197, must conform to the Oregon Statewide Planning Goals and maintain an implementing ordinance, through which the Roseburg Municipal Code carries out the goals and policies of the Comprehensive Plan. The intent of these ordinance amendments are to maintain consistency with ORS as the State makes changes from time to time and to comply with the intent of the Comprehensive Plan.

Comprehensive Plan

Pertinent policies that apply to the proposal have been evaluated as follows:

Transportation Policy No. 1

The City will continue to develop and refine street standards as necessary, particularly for local streets where site-specific characteristics are most important. Flexibility in the design of local streets shall be encouraged.

Finding:

Section 12.06.020(C) of the Roseburg Municipal Code currently requires developers to submit a Traffic Impact Study (TIS) to demonstrate the proposed development will not negatively impact existing transportation systems. To prove this, the TIS is evaluated against the City's adopted mobility standard. As part of the 2019 Transportation System Plan (TSP) update, it was recommended that the City adopt an alternative mobility standard utilizing a Level of Service (LOS) "E" and a volume/capacity (v/c) ratio of 0.95. The current amendments call for amending the existing standard by approximately 5% for collector streets and by 10% for arterial streets. This change would align with this policy by allowing for some additional flexibility in the existing capacity measures for vehicles. The LOS and v/c amendments do not anticipate any effect to existing bicycle, pedestrian, or utility infrastructure within the existing right-of-way.

Transportation System Plan

Pertinent Transportation System Plan policies that apply to the proposal have been evaluated as follows:

Transportation Options

- As development occurs, maintain a network of arterials, collectors, local streets, and paths that are interconnected, appropriately spaced, and reasonably direct.
- Ensure neighborhood and local connections provide adequate circulation into and out of neighborhoods.

Finding:

The intent of the proposed amendments is to provide the opportunity to allow more capacity on existing city streets. However, improvements, street widening, or street extensions shall still occur if an arterial, collector street or intersection is negatively impacted by development to the point where the future traffic impacts will exceed either the proposed LOS or v/c ratio. Local classified streets are not anticipated to be impacted by the proposed amendments as their mobility standard is not changing.

Implementation

- Plan for an economically viable and cost-effective transportation system.

Finding:

Current code requires property developers to mitigate negative traffic impacts such as vehicle delay during the peak hour by providing street enhancements such as signal upgrades, medians, and access management solutions. In addition, the City is responsible for major upgrades to intersections and street corridors that exceed the mobility standard. These street upgrades are recommended and tracked in the City's capital improvement plan which pulls from the updated TSP project list which provides a 20 year planning horizon for transportation projects. During the update of the TSP, the projects that were recommended were all based on traffic growth using the proposed LOS and v/c mobility standard. The reason for this is because the City guided the consultant to provide a more economical projection of future projects the City could plan for. Therefore, updating the TSP to accommodate a more economical projection of future street projects and allowing a slight increase in allowable capacity of existing roadways will align these amendments promoting a cost-effective and economically viable transportation system.

Statewide Planning Goals

Pertinent Statewide Planning Goals that apply to the proposal have been evaluated as follows:

Statewide Planning Goal # 1 - Citizen Involvement

To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

The City of Roseburg and Douglas County have an adopted and acknowledged Comprehensive Plan for the Roseburg Urban Area. The Comprehensive Plan is implemented via the adopted Code, in which the City identifies procedural

requirements for processing land use actions, including notification and hearing procedures. The notice procedures guide the general public through the land use process within the City as well as through provisions that meet Oregon Revised Statutes (ORS).

Roseburg also has an established Planning Commission that has the responsibility to act as the conduit to the City Council on land use matters. The Planning Commission is selected through an open, well-publicized public process and the Commission may include one member who resides outside the city limits. All meetings were advertised to local media. The proposed amendments were the result of input from the public who were affected by the Transportation System Plan update, Staff, and members of the Public Works Commission and Planning Commission members.

The City of Roseburg provided notice of this proposal as mandated through ORS and Municipal Code requirements, as well as publishing the notice in the News Review, a newspaper of general circulation. A public hearing(s) is held in order to provide an opportunity for interested citizens to be involved, provide comments and present issues, influence the Commission and eventually the Council, provide technical information, and/or provide information regarding conditional approval.

Statewide Planning Goal # 2 - Land Use Planning

To establish a land use planning process and policy framework as a basis for all decisions and actions related to the use of land and to assure an adequate factual base for such decisions and actions.

As noted above the City of Roseburg has adopted a Comprehensive Plan, which is "acknowledged" by the State of Oregon. This Plan was again acknowledged through Periodic Review in 1992 and is coordinated and adopted by Douglas County for the unincorporated area located within the City UGB. (Roseburg Urban Area Comprehensive Plan adopted by the City Council in Ordinance No. 2345, effective on July 1, 1982, and re-adopted in Ordinance No. 2980 on December 9, 1996.) Implementation of the Comprehensive Plan is accomplished through the adopted Code. The Land Use and Development Regulations Chapter of Roseburg Municipal Code has been acknowledged by the State of Oregon and has been amended from time-to-time in order to comply with ORS. (Roseburg Land Use and Development Ordinance No. 2363, as originally adopted July 1, 1984) Both the Comprehensive Plan and LUDR have been amended from time-to-time.

Statewide Planning Goal #12 – Transportation Planning

To provide and encourage a safe, convenient and economic transportation system.

The City of Roseburg has recently adopted a Transportation System Plan together with cooperation from the Oregon Department of Transportation (ODOT), which is the reviewing state agency that determines consistency with the Transportation Planning Rule (TPR) and Statewide Planning Goal 12. The TSP, in coordination with ODOT, was drafted under the guidance of OAR 660-015-0000(12), Goal 12 guidelines, and other applicable administrative rules and statutes relating to development and adoption of a Transportation System Plan in order to fully comply

with all applicable state requirements, while still tailoring the TSP to meet and address the transportation needs of the citizens of Roseburg. In addition, periodic coordination with ODOT occurred prior to, during, and after completion of the project to ensure consistency with the TPR.

IV. CONCLUSION

Based on the above findings, the Planning Commissions concludes that the application meets the criteria for approval in RMC 12.10.020(F)(2).

V. ORDER

Based on the Findings and Conclusions above, the Planning Commission recommends approval of this application to the City Council.

Planning Commission Chair	Date	
Stuart Cowie, Community Development Director	Date	

Planning Commission Members: Shelby Osborn Dan Onchuck Ronald Sperry Victoria Hawks Stephanie Newman Jaime Yraguen Christy Palmini

Text Amendments

RMC 12.06.020(C)

- C. Traffic impact study.
- 1. A Traffic Impact Study (TIS) shall be required based on anticipated negative significant traffic and safety impacts projected to be caused by the proposed development as determined by the Community Development Director after a recommendation from the Public Works Director.

Negative significant traffic and safety impacts may include, but not be limited to:

- a. An anticipated increase of at least five percent (5%) of the current traffic volume during the peak hour and at least 100 trips per day.
- b. Additional traffic and turn movement projected to result from the proposed development is projected to exceed the applicable volume to capacity ratio (V/C) and/or level of service (LOS) mobility standards:

Street Functional	V/C ¹	LOS ²
<u>Classification</u>		
Arterial, Collector and Local	0.95	E

Volume to Capacity Ratio:

Arterial: 0.85

Collector: 0.90

Local: 0.95

Level of Service Standard:

Signalized intersection: LOS D

Non-signalized intersection: LOS E

Downtown Intersection: 0.95 and LOS E

- c. A significant capacity and/or safety problem is likely to be caused by, or increased by the development.
- 2. When required, the TIS shall, at a minimum:
- a. Utilize a Scope of Work and an Analysis Methodology approved or accepted by the Community Development Director.
- b. Consider cumulative impacts of existing and proposed development in the study area.

¹ City intersections shall be analyzed at a peak hour factor of 1.0.

² For roadways within the City of Roseburg that are under ODOT or Douglas County jurisdiction, the mobility standards of those agencies will apply.

- c. Include long-term impact (20-year) of the development in the context of the projected traffic environment at five-year increments.
- d. Consider circulation and safety needs for pedestrians, bicyclists, and transit in addition to motor vehicles.
- e. Extend the analysis coverage of the street system until the peak traffic impact becomes less than five percent (5%).